

13.1 GENERAL APPLICATION PROCESS

Access requests to County streets and roadways are processed through one of the following methods:

- A. Planned Developments may set general locations for access points.
- B. For new subdivisions, access is granted through the Platting process.
- C. For commercial, industrial and multi-family developments, access is granted through the SIP process.
- D. For existing zoned property, such as Agricultural, and Locations and Extent projects, application for access can be made using the Engineering Division’s Access Permit Application Submittal Form identified in Chapter 16. This application should be accompanied by plans of the proposed access, technical justification for the access and associated public improvements.
- E. All single family residential driveway permits shall be issued through the County Building Division.

In some circumstances the County may require an Access Permit for subdivision, commercial, industrial or multi-family access.

Typically access shall not be granted to County ROW that has not been improved to County standards and not maintained by Douglas County.

The Engineering Division and/or Building Division staff is available to provide assistance on the extent of technical justification required for any access request. It is recommended that this assistance be sought prior to submitting any application.

13.2 CRITERIA FOR ACCESS ONTO ROADS WITHIN DOUGLAS COUNTY

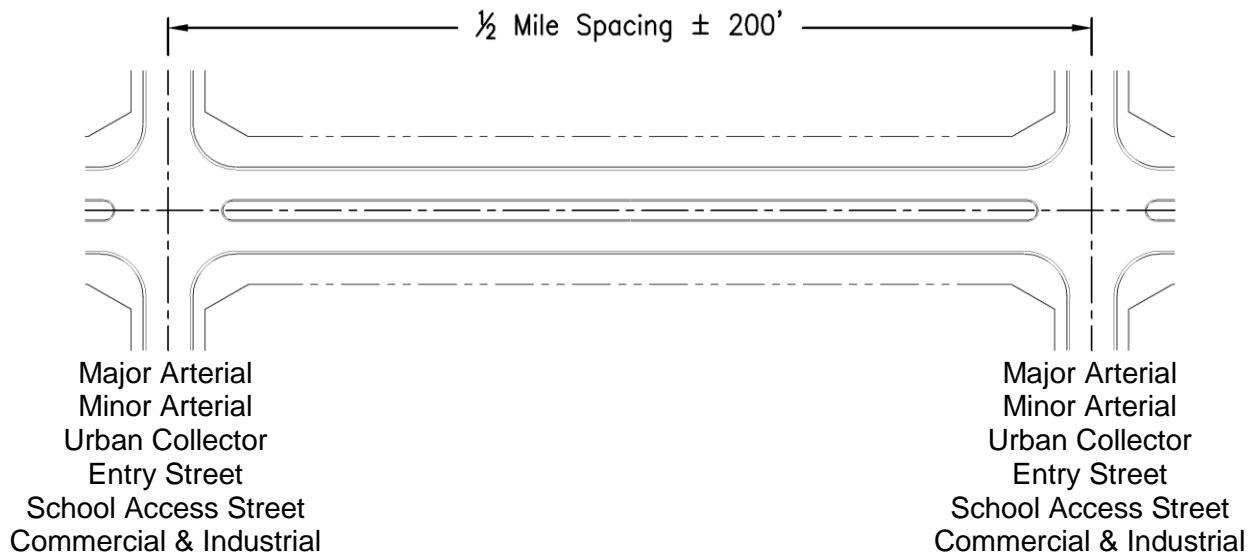
13.2.1 Freeways

All new freeway access in Douglas County shall meet the requirements of the Colorado Department of Transportation or the appropriate governing authority.

13.2.2 State Highways

Access to State Highways is governed by the State Highway Access Code; however Douglas County is the issuing authority for the State Highway Access Permits within unincorporated Douglas County.

13.2.3 Major Arterials



13.2.3.1 Private Access

Generally, no private full movement access shall be allowed onto a Major Arterial unless a signal progression plan has been reviewed and accepted by Douglas County Engineering. Private direct access to Major Arterials may be permitted only when the property in question has no other reasonable access to the general street system, or when denial of direct access to the Major Arterial and alternative direct access to another roadway would cause unacceptable traffic operation and safety problems to the overall traffic flow of the general street system. When private access must be provided, the following shall be considered:

- A. Such access shall continue only until such time that some other reasonable access to a lower function category street is available and permitted. The Access Permit should specify the future reasonable access location(s), circumstances, and required changes.
- B. No more than one access shall be provided to an individual parcel or to contiguous parcels under the same ownership unless it can be shown that (1) allowing only one access conflicts with safety regulations (i.e. fire access), or (2) additional access would significantly benefit safety and operation of the highway or street and is necessary to the safe and efficient use of the property.
- C. An access shall be limited to right-in/right-out movements only, unless (1) it has the potential for signalization, and (2) left turns would not create unreasonable congestion or safety problems and lower the level of service. In certain situations the access may be further limited.

13.2.3.2 Public Access

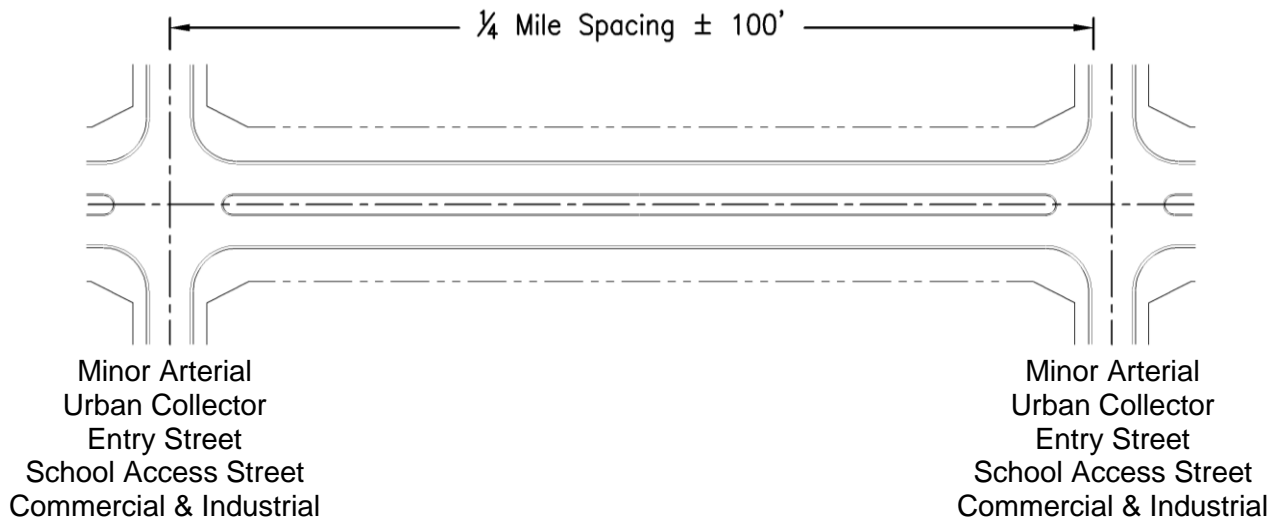
Public direct access to a Major Arterial, where left turns are to be permitted must meet the County's signal spacing criteria. Those that do not meet these requirements shall be limited to right-in/right-out movements only, unless they

meet the requirements of 13.2.3.1.C above. No local streets shall be permitted to intersect Major Arterials.

Full movement access to Major Arterials shall be limited to ½ mile intervals, plus or minus 200', in order to achieve good speed, capacity, and optimal signal progression. To provide flexibility for both existing and future conditions, an accepted engineering analysis of signal progression shall be made to properly locate any proposed access that may require signalization. The specifics of this analysis are detailed in the "Traffic Impact Study Criteria" (See Appendix B).

If the access location is not accepted through the SIP process, an Access Permit must be obtained from the County for any private access requested to a Major Arterial.

13.2.4 Minor Arterials



13.2.4.1 Private Access

Generally, no private full movement access shall be allowed to a Minor Arterial. Private direct access to a Minor Arterial may be permitted if:

- A.** Such access shall continue only until such time that some other reasonable access to a lower function category street is available and permitted. The Access Permit should specify the future reasonable access location(s), circumstances, and required changes.
- B.** The access has potential for signalization per the requirements of Section 13.2.3.1.B and 13.2.3.1.C above.
- C.** The access meets the signal spacing requirements for intersecting public streets and does not interfere with the location, planning, safety and operation of the general street system and access to nearby properties.

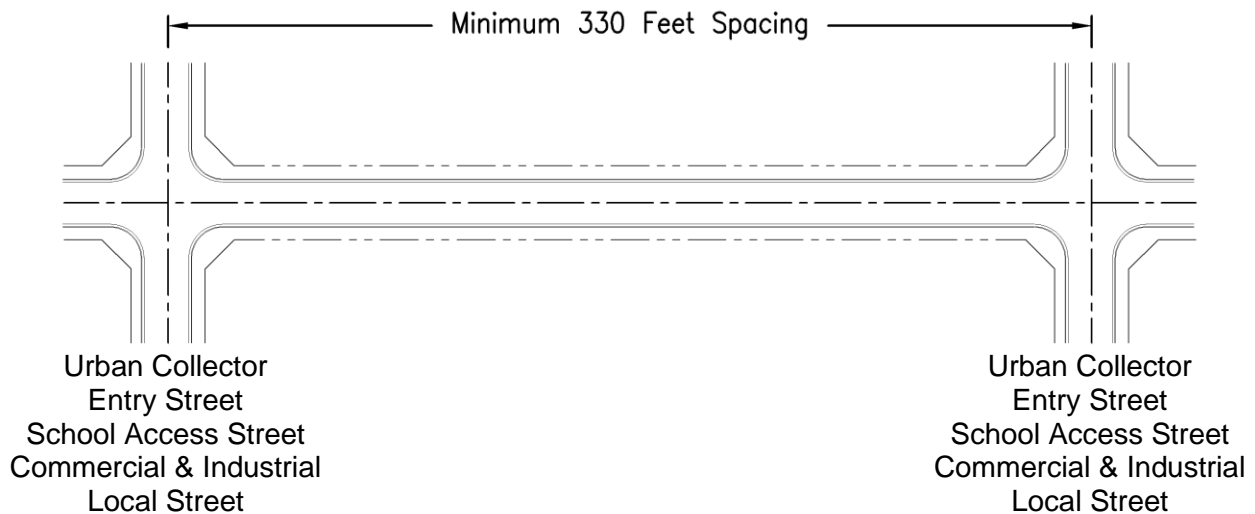
13.2.4.2 Public Access

Public direct access to a Minor Arterial, where left turns are to be permitted must meet the County’s signal spacing criteria. Those that do not meet these requirements shall be limited to right-in/right-out movements only, unless they meet the requirements of Section 13.2.3.1.C above. No local streets shall be permitted to intersect Minor Arterials.

Full movement access to Minor Arterials shall be limited to ¼ mile intervals, plus or minus 100’, in order to achieve good speed, capacity, and optional signal progression. To provide flexibility for both existing and future conditions, an accepted engineering analysis of signal progression shall be made to properly locate any proposed access that may require signalization. The specifics of this analysis are detailed in the “Traffic Impact Study Criteria” (See Appendix B).

If the access location is not accepted through the SIP process, an Access Permit must be obtained from the County for any private access requested to a Minor Arterial.

13.2.5 Urban Collectors



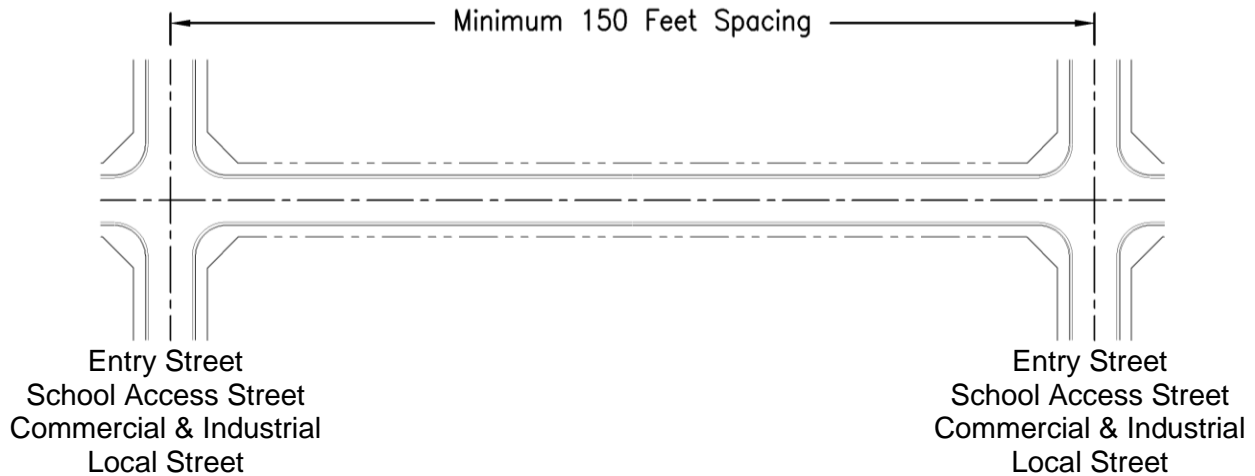
Private access to Urban Collectors shall be governed by the following curb opening and driveway criteria. Single family residence access to Collectors is not permitted unless access to a lower function category street is not available.

Streets shall intersect Urban Collectors not closer than 330’ from each other (centerline to centerline).

The closest Local Street intersection to an Arterial along an Urban Collector shall be 330’ from the Arterial ROW to the centerline of the Local Street. See Figure 13-1.

If the access location is not accepted through the SIP process, an Access Permit must be obtained from the County for any private access requested to a Minor Urban Collector.

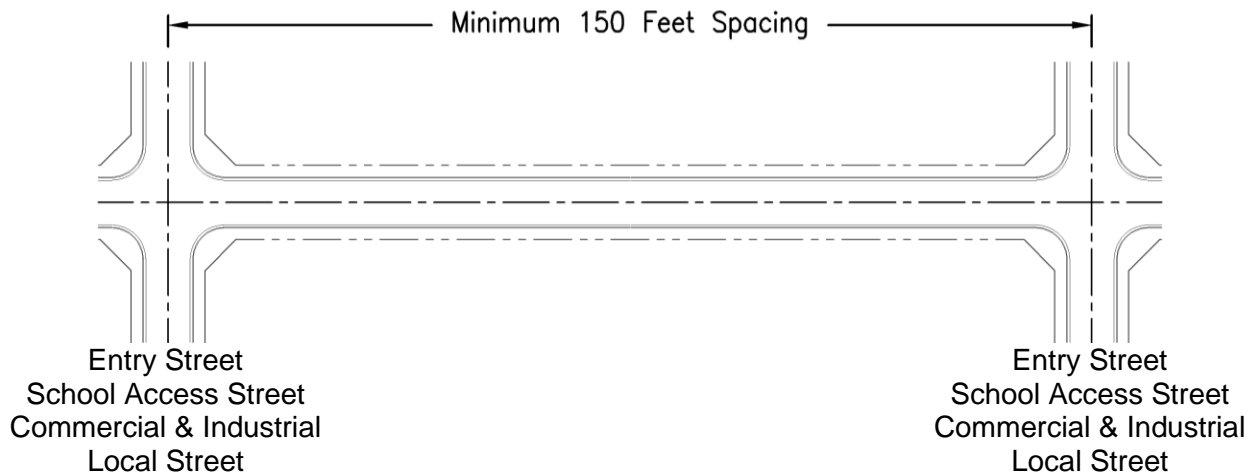
13.2.6 School Access Street



Private access to a School Access Street shall be governed by the following curb opening and driveway criteria:

- A.** Streets should not intersect a School Access Street closer than 150' from each other (centerline to centerline). On a School Access Street, the closest intersection to a Minor Urban Collector street shall be at least 200' (centerline to centerline).
- B.** No signalization shall be allowed on a School Access Street.
- C.** An Access Permit must be obtained from the County for any access other than single family requested to a School Access Street.

13.2.7 Commercial and Industrial Street



Private access to a Commercial and Industrial Street shall be governed by the following curb opening and driveway criteria:

- A.** Streets shall intersect Commercial & Industrial Streets not closer than 150' from each other (centerline to centerline). The closest Local Street

intersection along a Commercial and Industrial Street to an Urban Collector shall be 200' (centerline to centerline) See Figure 13-1.

- B.** Signalization shall not be allowed on a Commercial and Industrial Street.
- C.** A Driveway Access Permit may be required from the County for any private access requested to a Commercial and Industrial Street.
- D.** In an effort to limit the number of "Points of Conflict" on Commercial and Industrial Streets, access to these streets should be limited to shared driveways located at common lot lines.

13.2.8 Entry Streets

Private access to Entry Streets is not allowed.

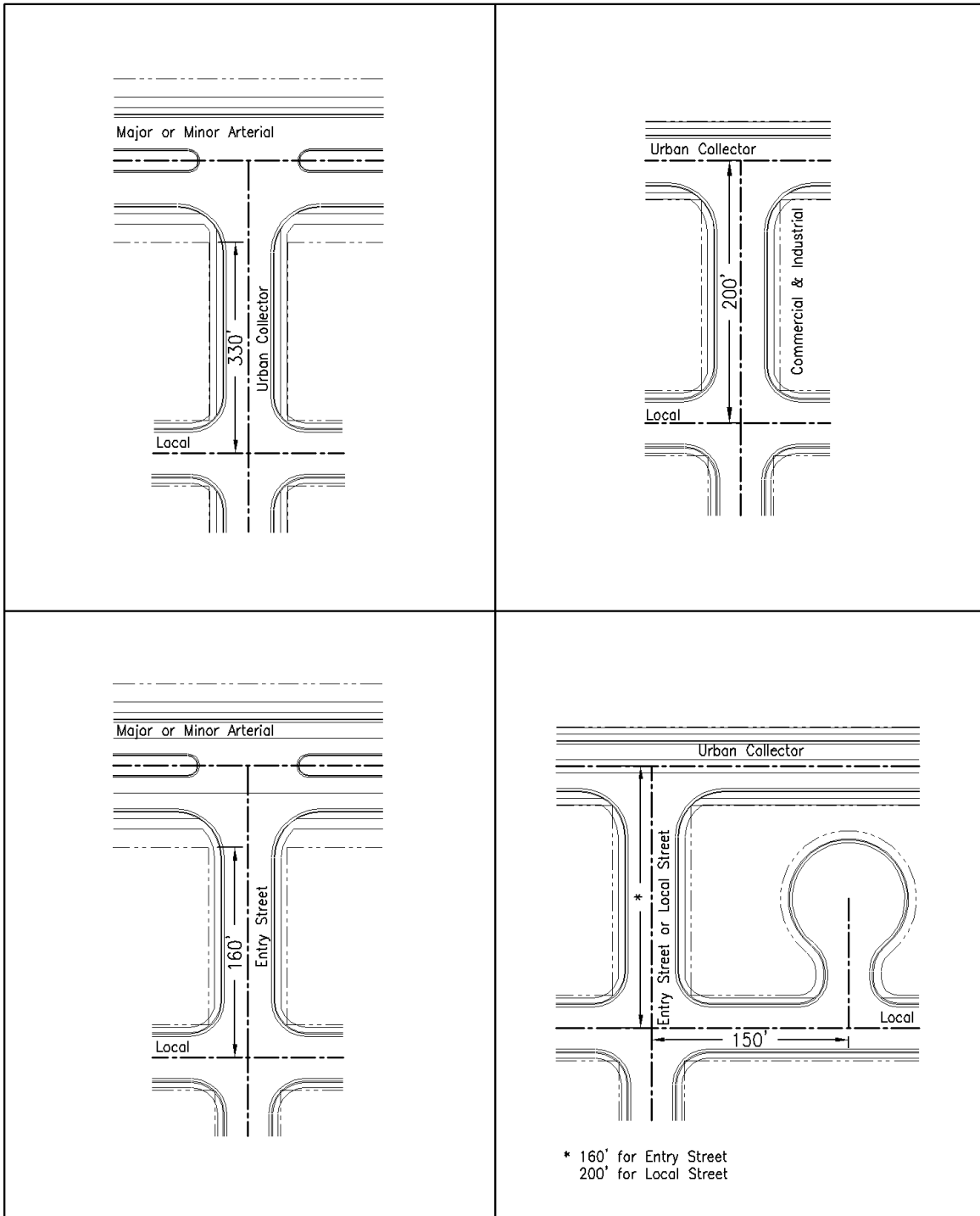
- A.** The closest Local Street intersection along an Entry Street to an Arterial shall be 160' (ROW line of Arterial to centerline of Local Street). The closest Local Street intersection along an Entry Street to an Urban Collector shall be 160' (centerline to centerline) See Figure 13-1.
- B.** Signalization shall not be allowed on an Entry Street.

13.2.9 Local Streets

Private access to local streets shall be governed by the following curb opening and driveway criteria:

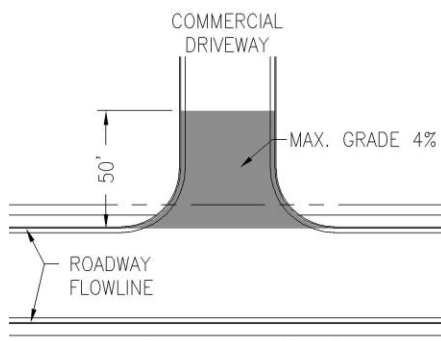
- A.** Local Streets shall intersect Local Streets not closer than 150' from each other (centerline to centerline). The closest Local Street intersection along a Local Street to an Urban Collector shall be 200' (centerline to centerline).
- B.** Signalization shall not be allowed on a Local Street.
- C.** Single family residential accesses to Local Streets are issued by the Building Division, except for lots less than ½ acre and/or the street has Combination Curb, Gutter & Sidewalk, in accordance with the County's driveway criteria.

**FIGURE 13-1
INTERSECTION SEPERATION FOR ENTRY STREET AND LOCAL STREETS**



13.3 BASIC PRINCIPLES FOR CURB OPENINGS AND DRIVEWAYS

- A. By controlling the location and width of curb openings or driveways along the street, it is possible to avoid or eliminate long open stretches where motorists can indiscriminately drive onto the street. In case of conflict between requirements in the various sections of this Chapter, the more restrictive will apply. The opening or driveway width should be adequate to properly handle the anticipated traffic volumes and character of traffic, as well as being within the limits specified for the type of property development.
- B. All openings for driveways shall be located at the point of optimum sight distance along the street. For openings and driveways to commercial establishments and service stations there shall be sufficient space cleared of any obstructions so drivers entering or exiting the property are given sufficient sight distance to enable them to make proper and safe movements. The profile of a driveway approach and the grading of adjacent area shall be designed so that when a vehicle is located on the driveway outside the travel lanes of the street, the driver can see a sufficient distance in both directions to enable the driver to enter the street without creating a hazardous traffic situation.
- C. Any adjustments which must be made to utility poles, street light standards, fire hydrants, inlets, traffic signs and signals, utility boxes or other public improvements or installations which are necessary as the result of the curb openings or driveways shall be accomplished without any cost to Douglas County. Any curb opening or driveway which has been abandoned shall be restored by the property owner to previous conditions.
- D. If the driveway is to serve as limited access to the property, the owner/developer will be required to provide some means of ensuring that the motorists will use the driveway as designed. This is typically done using a raised median in the through street. If access is limited at the driveway location (Right-In/Right-Out or Three Quarters Movement) the island design shall follow AASHTO design criteria.
- E. The County may limit or modify access points if traffic problems occur or if road improvements necessitate.
- F. The length of the maximum grade of 4% along centerline for a commercial driveway shall be a minimum of 50' measured from the flowline intersection of the roadway.

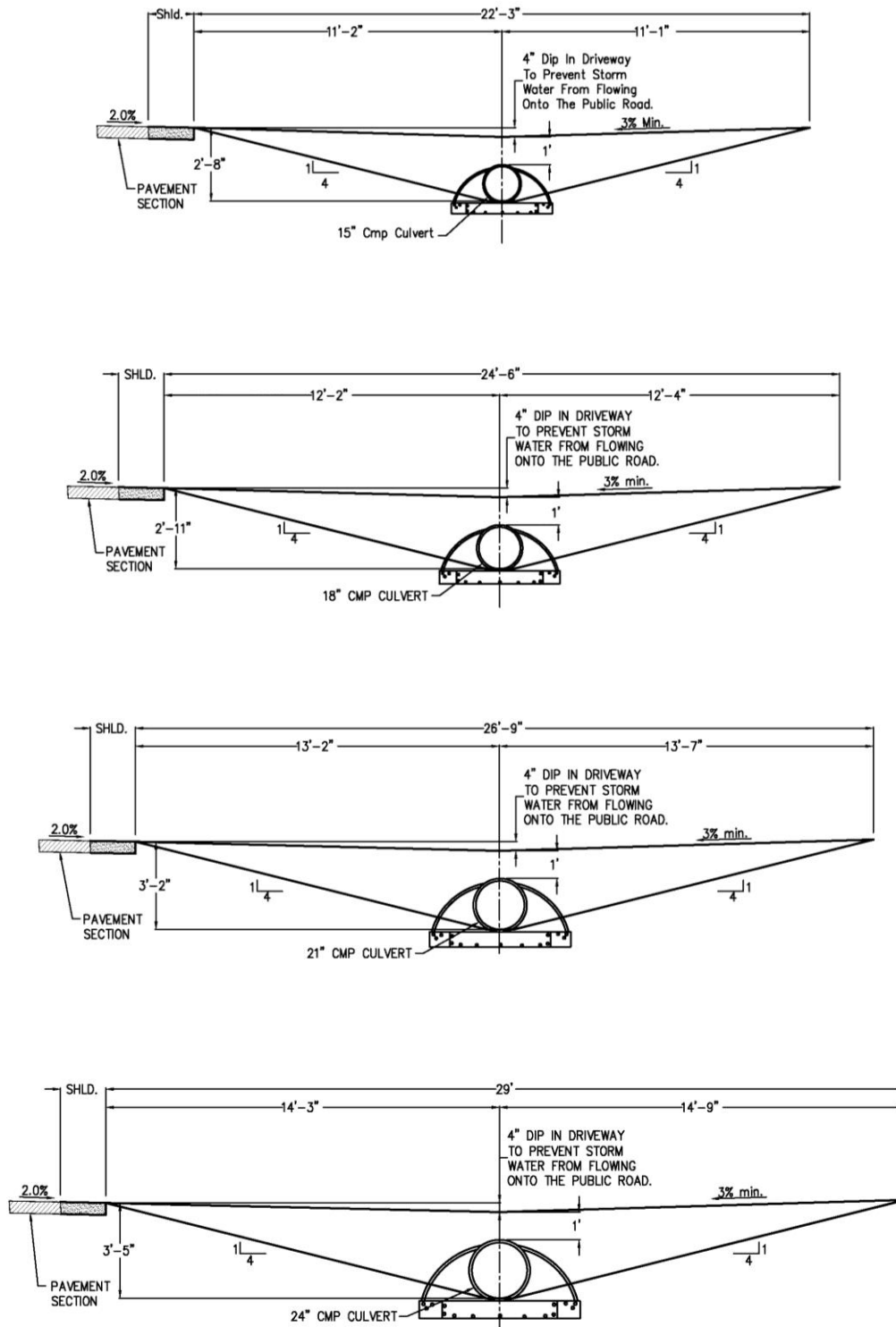


13.4 PRIVATE DRIVE ACCESS TO RURAL ROADWAYS

Private drive access to rural roadways that have no curb and/or gutter improvements shall be constructed to meet the following requirements:

- A.** Drive shall extend from ROW line to edge of existing driving surface and shall be constructed using the following criteria:
 - A 6-inch thick compacted class 6 aggregate base material shall be used if the existing driving surface of the County Road is gravel.
 - A minimum 3-inch thick asphalt pavement over 6-inch thick class 6 aggregate base material or minimum of 5-inch full depth asphalt if the existing driving surface of the County Road is paved.
 - If the driveway is for Agricultural use (no dwelling units), the surface of the driveway may be Class 6 aggregate base course as described above.
- B.** The drive entrance shall be a minimum of 12' wide (maximum 30') in the County ROW.
- C.** A minimum 15-inch diameter corrugated annular metal pipe (CMP) culvert with flared end sections, shall be installed at the established roadside ditch flowline beneath the private drive access with a minimum 12-inches of cover over the culvert.
- D.** An exhibit of installation must be submitted with the access permit application. CMP culverts are only allowed for private residential driveways, not for storm sewer or commercial driveways. See Figure 13-2, Minimum Driveway Dimensions Based On Required Culvert Size.
- E.** Maintenance of the access and drainage improvements related with the access within County ROW shall be the responsibility of the adjacent property owner.

**FIGURE 13-2
MINIMUM DRIVEWAY DIMENSIONS BASED ON REQUIRED CULVERT SIZE**



13.5 GENERAL REQUIREMENTS

13.5.1 Number of Driveway Openings

- A. Single Family (SF) Residential – Single Family residential property shall be limited to one access point.
- B. Multi Family (MF) Residential, Commercial and Industrial – If located on a Commercial and Industrial or Collector roadway, use the information provided in Figure 13-1. If access is located on a roadway classified other than Commercial and Industrial, driveway openings shall be determined on a case-by-case basis based on intersection spacing criteria of a Traffic Impact Study accepted by Douglas County.

13.5.2 Driveway Entrance Angle

The driveway entrance angle for all approaches shall be $90^{\circ} \pm 10^{\circ}$ to the centerline of the street. See Figure 4-12.

13.5.3 Joint Driveway Access

Joint driveway access must be considered to serve two adjacent properties for Commercial and Industrial Developments. Joint driveway accesses are typically centered on the common property line and cross-access easements are required.

13.5.4 Minimum Space Between Driveway Openings (D)

If Joint Access is not used, the minimum spacing between driveways shall be 50' measured between the inside edges of the two adjacent driveways. New driveways shall align with existing driveways on the opposite sides of the street or be offset as noted above. This does not apply to single family residential projects, see Figure 13-3.

13.5.5 Driveway Movements

Projects shall be designed in order to prohibit backing maneuvers in the ROW. Driveways accessing roadways shall not allow parking within 50' from edge of adjacent road.

13.5.6 Change In Use Or Traffic Volume

If the use changes or volume increases by 20% of an existing access to County ROW, or if there is a change in the use or developed size of the property, a new Access Permit and additional improvements may be required and/or the access may be limited.

Change in access or property use may include, but is not limited to, change in the amount or type of traffic, structural modifications, remodeling, change in type of business, expansion in existing business, change in zoning, change in property division creating new parcels, etc.

13.5.7 Auxiliary Lanes (Acceleration/Deceleration Lanes)

See Chapter 4 of these *Roadway Criteria*.

13.6 ACCESS DESIGN

13.6.1 Width of Driveway Opening (W)

The total width of driveway opening for properties measured at the curb line for various Street Classifications shall be in conformance with Table 13-1.

**TABLE 13-1
WIDTH OF DRIVEWAY OPENINGS**

	RESIDENTIAL		COMMERCIAL	INDUSTRIAL
	SF	MF		
MAJOR ARTERIAL	N/A	38' (if allowed)	38' (if allowed)	38' (if allowed)
MINOR ARTERIAL	N/A	38' (if allowed)	38' (if allowed)	38' (if allowed)
URBAN COLLECTOR	N/A	38'	24'-36'	24'-36'
LOCAL	(**)	38'	24'-36'	24'-36'

**for single-family residential access to local streets:

Local (Urban) = 16' to 30'

Local (Rural - less than 35 acres) = 16' to 22'

Local (Rural - 35 acres and larger) = 18' to 24'

NOTE:

1. Driveway openings of 30' or more must be constructed as a radius curb return.
2. If a radius curb return is used, the width of the allowed driveway does not include the curb return radius.
3. The width of the driveway opening does not include the tapers, see Curb Opening detail in Appendix A.
4. The dimensions shown may need to be increased based on a Traffic Impact Study accepted by Douglas County.

13.6.2 Driveway Edge Clearance (E)

The distance measured along the curb line from the nearest edge of the curb opening to a point where the property line is extended to the curb line. See Figure 13-3.

NOTE: Joint access with adjoining property must be considered. Joint access shall be the only justification for eliminating the minimum edge clearance dimension.

13.6.3 Corner Clearance (C)

The distance measured along the curb line from the nearest edge of the curb opening to the adjacent street ROW line. See Figure 13-4.

It is important to locate driveways away from major intersections. This constraint is as much for the ability to enter and leave the property as for the benefit of intersection safety and operations. Exiting a driveway during peak-hour conditions at traffic signals is difficult where the queue of standing or slow-moving vehicles does not allow a sufficient gap for entry from the driveway.

13.6.4 Sight Distance

Sight distance for curb openings to private property shall consist of a sight triangle conforming to the requirements of Section 4.11.4 of these *Roadway Criteria*.

13.7 UNPERMITTED ACCESS

Any access, driveway, or curb-cut which is constructed within Public ROW without an access permit issued by Douglas County or by an accepted site plan shall be subject to a “Stop Work” order and shall be removed immediately. Failure to obey the “Stop Work” order may result in the prosecution of the violators. Failure to remove the unpermitted access may result in the removal of said access by Douglas County and the cost of removal shall be charged to the property owner from which the access originates. In the event the property owner fails to reimburse Douglas County within thirty (30) days after submission of the bill for the costs incurred, Douglas County shall have the right to enforce such obligation by appropriate legal action.

**FIGURE 13-3
CURB OPENINGS AND DRIVEWAY SPACING**

NOTE:

Single family residential driveways must access on the street of the lowest classification and at the side of the lot furthest away from the intersection. Any deviations from this criteria requires acceptance through Engineering.

These dimensions only apply to the road classifications shown; if access is to a different classification of road then intersection spacing shall apply.

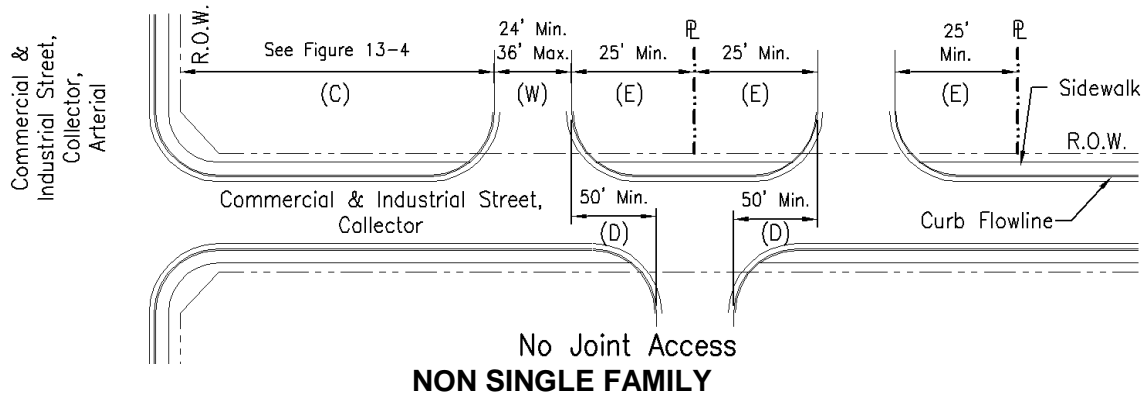
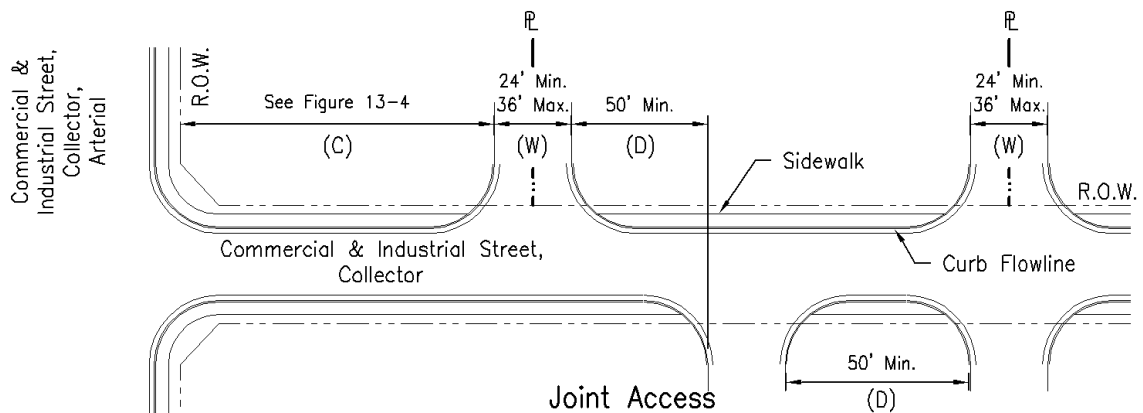
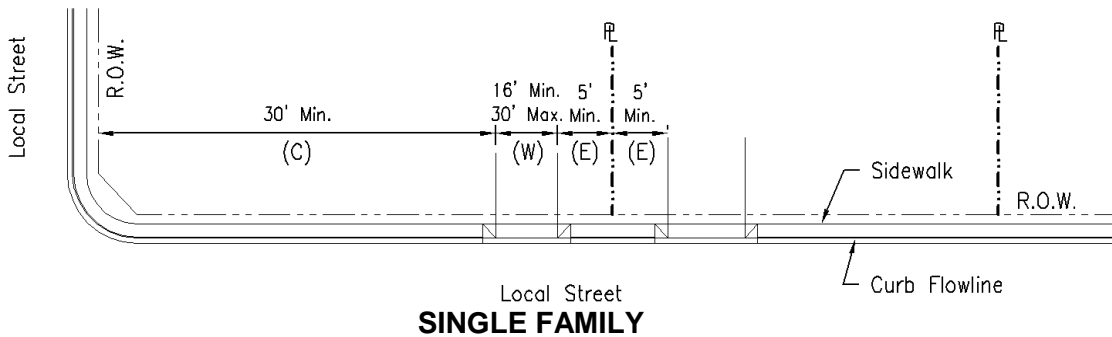


FIGURE 13-4
CORNER CLEARANCES FOR NON SINGLE FAMILY DRIVEWAYS
 CORNER CLEARANCES

MINIMUM CORNER CLEARANCES			
ITEM	CLASS OF MAJOR ROADWAY		
	ARTERIAL	COLLECTOR	LOCAL
A	115'	75'	50'
B	85'	85'	50'
C	115'	75'	50'
D	115'	75'	50'
E	210'	115'	50'
F	210'	115'	50'
G	50'	50'	50'
H	210'	115'	50'

NOTE:

This table does not apply to single family residential driveways. Single family residential driveways must access on the street of the lowest classification and at the side of the lot furthest away from the intersection.

