

DOUGLAS COUNTY INTEGRATED TRANSIT AND MULTIMODAL STUDY

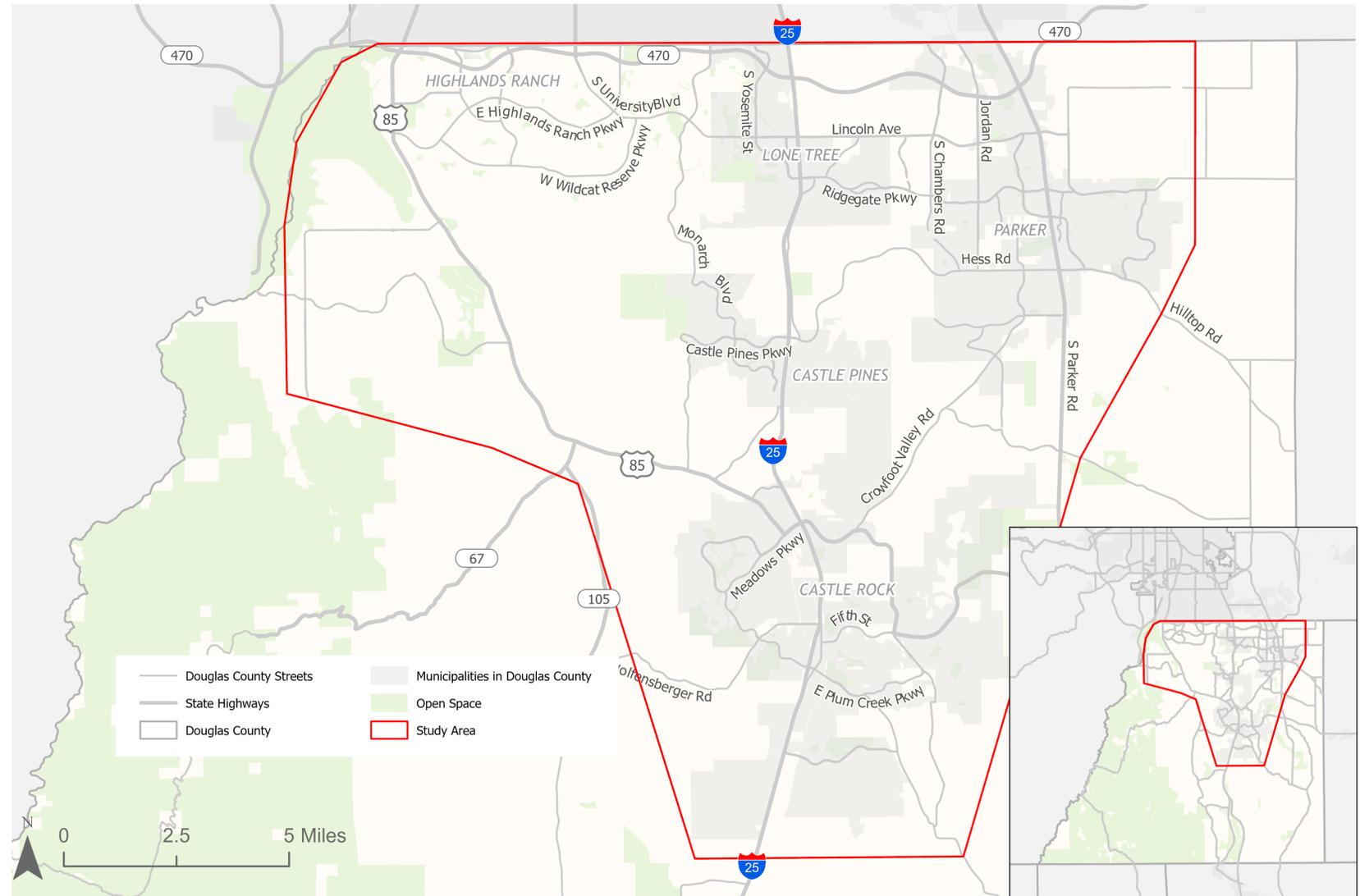


This effort is to improve existing transit services and find ways to provide reasonable and reliable transit to people who do not own or have access to their own transportation, have limited personal mobility, or want to use transit in their commute to work, school, and services. The study focuses on the northern area of Douglas County, including the City of Castle Pines, the Town of Castle Rock, Highlands Ranch, the City of Lone Tree, and the Town of Parker.

PROJECT GOALS

1. Find short- and long-term projects to help people move around the county more easily and efficiently, including to jobs, doctors' appointments, and schools
2. Connect people to rail stations, transit hubs, and major activity centers through transit opportunities
3. Create a transportation system that increases options for getting around
4. Define bicycle and pedestrian connections that support and encourage transit use
5. Strengthen collaboration between Douglas County municipalities

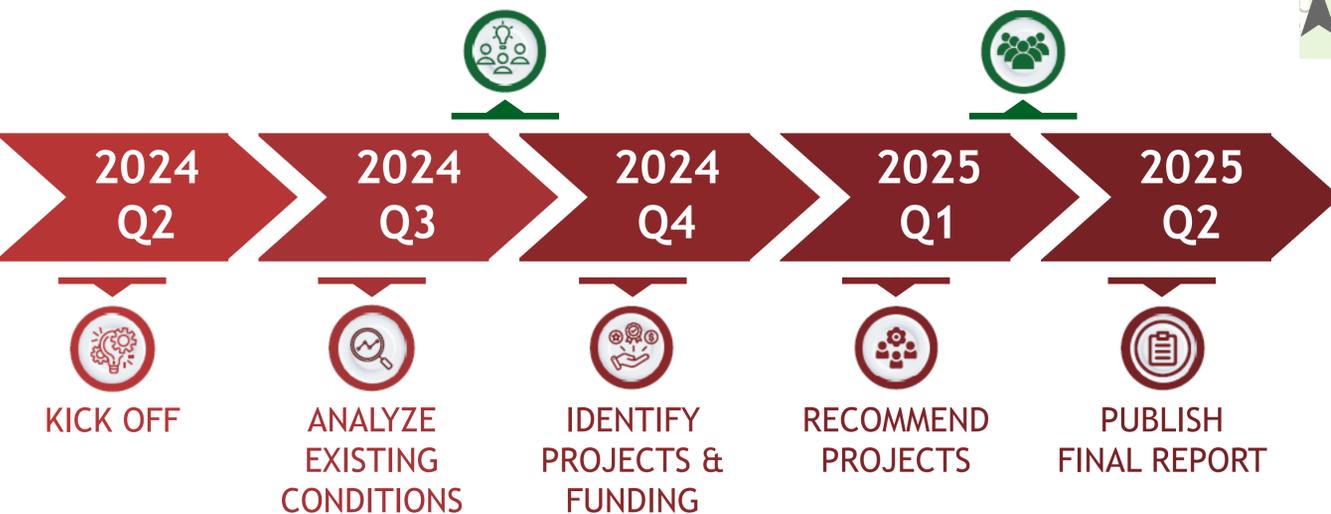
Drop a pin near where you live



TIMELINE

FOCUS GROUPS

PUBLIC OPEN HOUSE



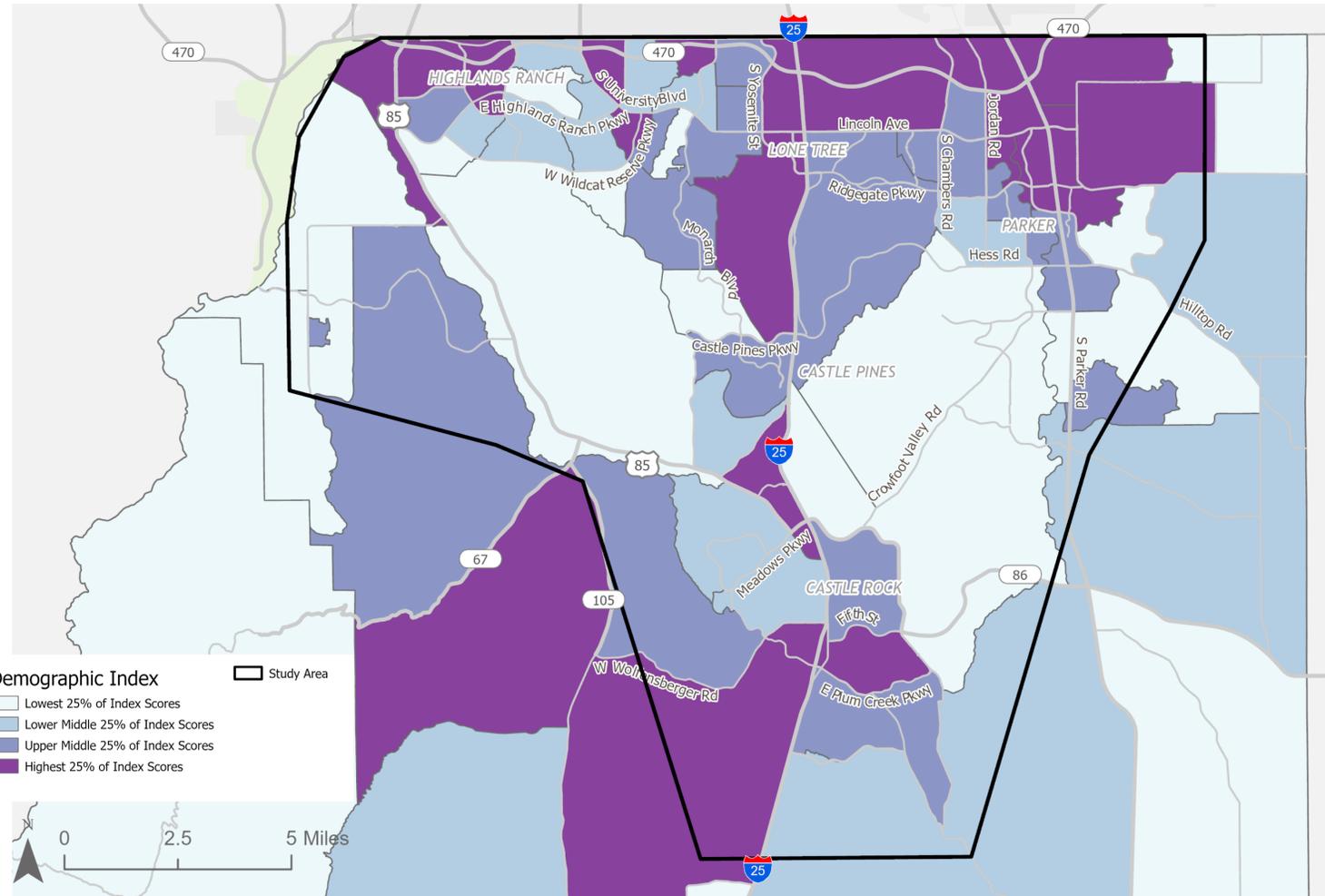
Scan code to learn more about the project here:



WHAT WE KNOW

EXISTING DEMOGRAPHIC PATTERNS

DRCOG DEMOGRAPHIC INDEX



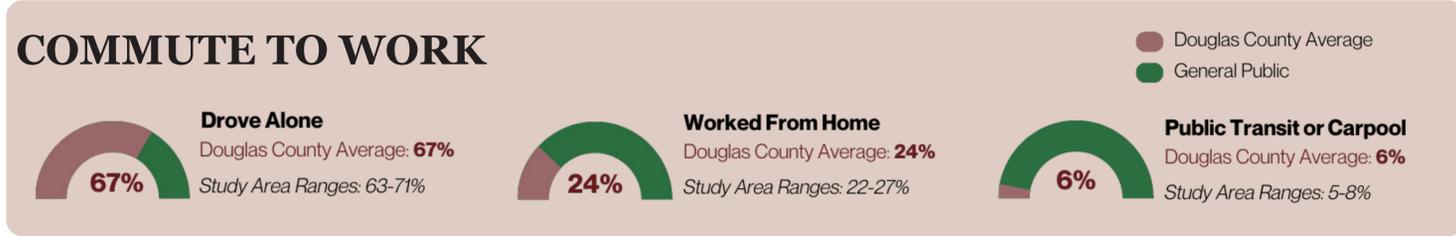
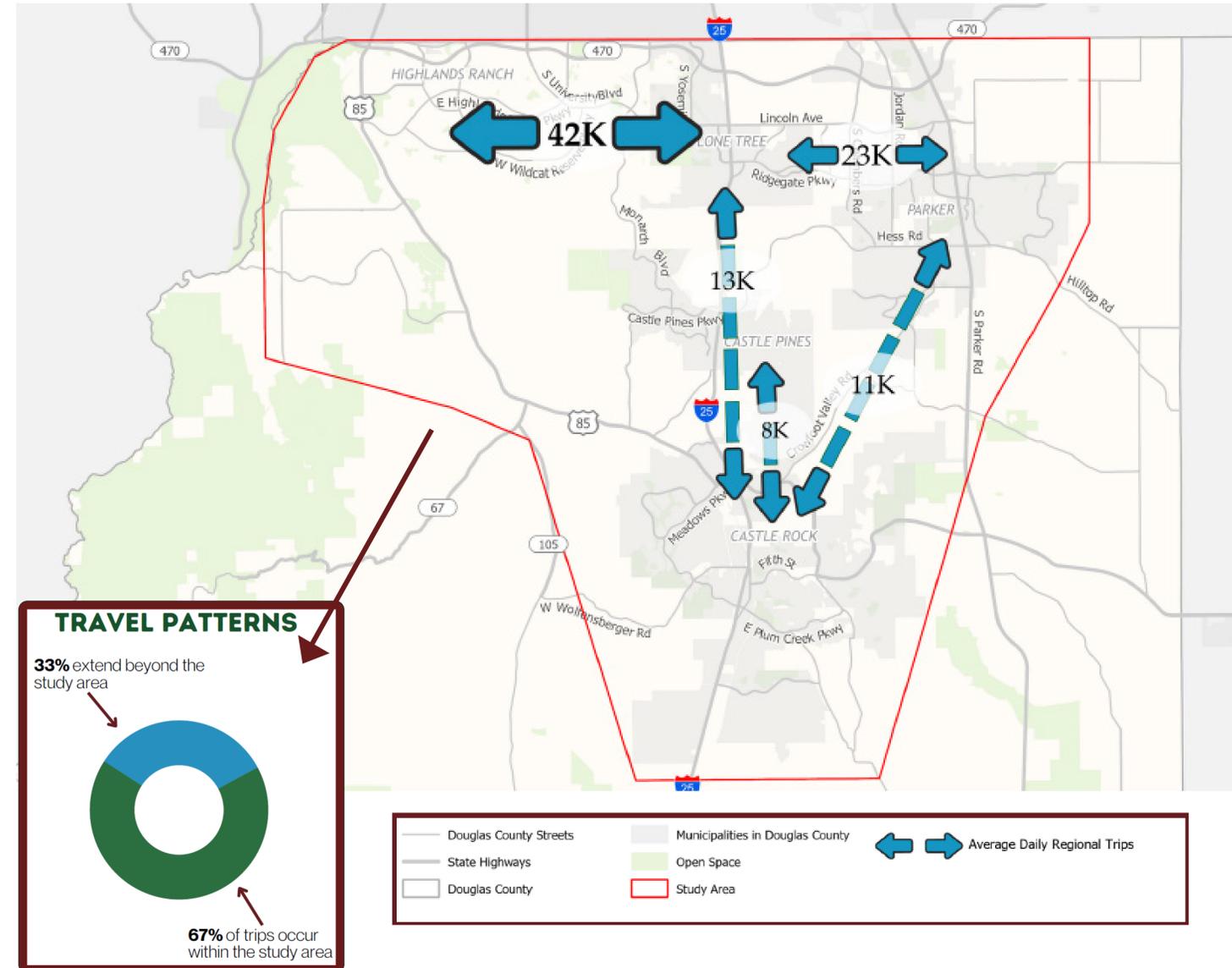
Darker colors indicate higher need and transit propensity

DRCOG DEMOGRAPHIC INDEX SHOWS COMMUNITIES WITH HIGHER NEED IN DOUGLAS COUNTY

- Adults 60 or older
- Youth under 18
- Limited English proficiency
- People of color
- People born outside the U.S
- Low Income
- Housing income-burdened households
- Single Parent Households
- People with disabilities
- Households without a vehicle

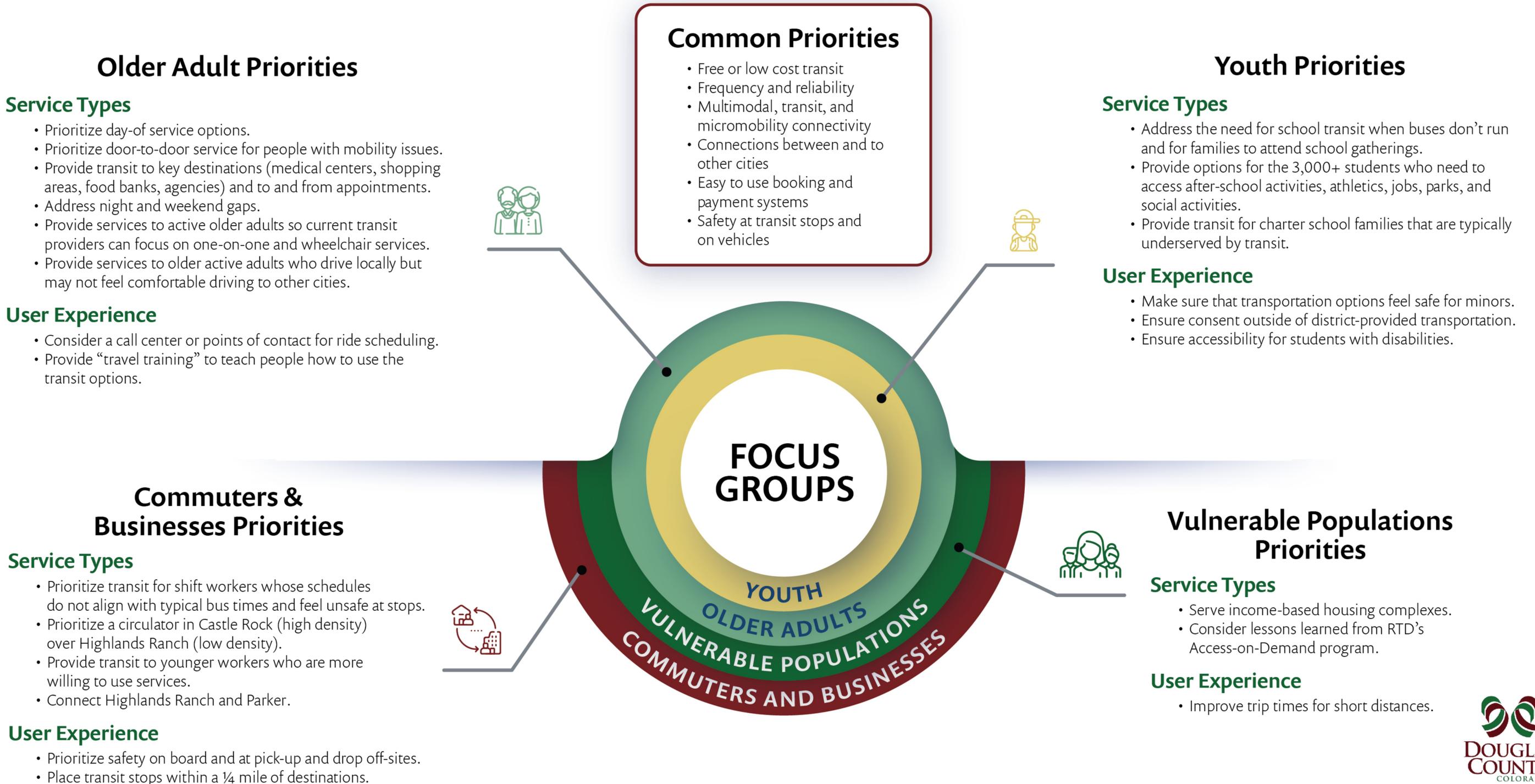
EXISTING TRAVEL PATTERNS

DAILY TRIPS WITHIN STUDY AREA



WHAT WE LEARNED FROM OUR FOCUS GROUPS

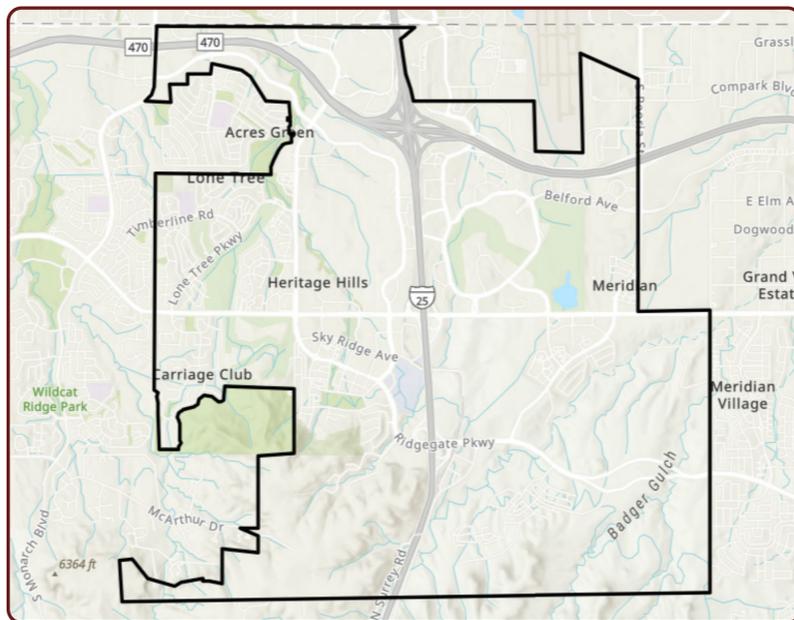
Douglas County Integrated Transit and Multimodal Study Focus Groups



TRANSIT TYPES FOR DOUGLAS COUNTY

MICROTRANSIT (ON-DEMAND)

People request an on-demand trip (through app or call center) connecting any two points within a defined zone



Link On Demand, Lone Tree, CO

ADVANTAGES

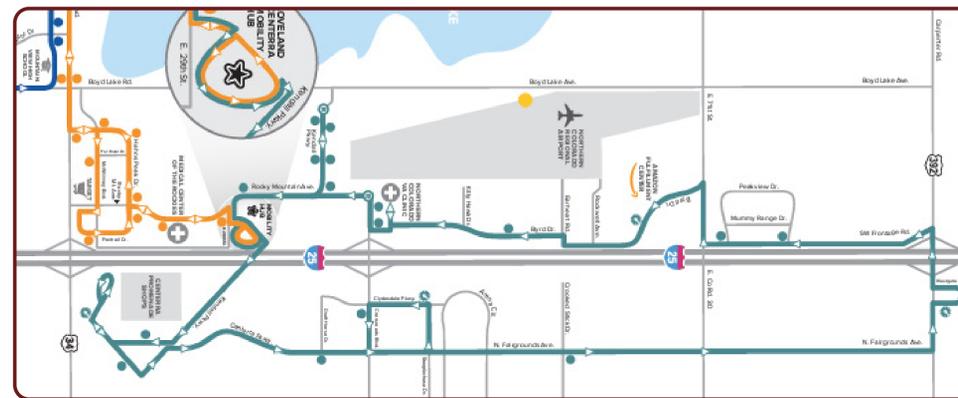
- Highly convenient, flexible, and responsive to riders and different trip purposes
- Adaptable and changeable over time
- More cost-effective for serving lower density, dispersed areas

DISADVANTAGES

- Operating costs are typically higher than fixed route
- As ridership increases, more/larger vehicles may be required to serve the same area
- Not necessarily a tool to reduce traffic or greenhouse gas emissions

LOCAL CIRCULATOR

Circuitous with travel times typically longer than travel by car, frequent stops, sometimes operated as a deviated fixed route (riders can request ¾ mile deviation from route)



COLT 7 Route, Loveland, CO

ADVANTAGES

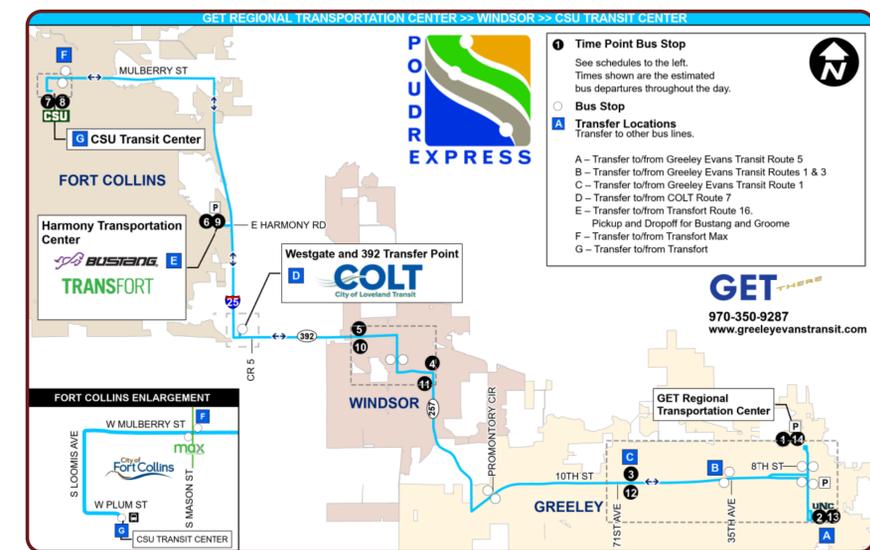
- Frequent stops allow limited walking distances to stops
- More frequent stops during peak periods can reduce walking distances
- Effectively serves short distance passengers with destinations just outside of comfortable walking/biking distance

DISADVANTAGES

- High number of stops, winding routes, and low-speed operations can result in long travel times for trips beyond 3 miles
- Several stops can impact travel time reliability
- Frequent stops can negatively impact travel time reliability
- A trade-off between access/frequency and speed
- Providing complementary ADA paratransit service can be costly

SUBREGIONAL FIXED ROUTE

Easy to understand direct routes, often paired with park-and-rides, travel times should be competitive with the time it takes to drive a private vehicle



Poudre Express Route, Greeley, CO

ADVANTAGES

- Typically provides frequent service during peak times
- Faster travel times
- Consistent on-time performance

DISADVANTAGES

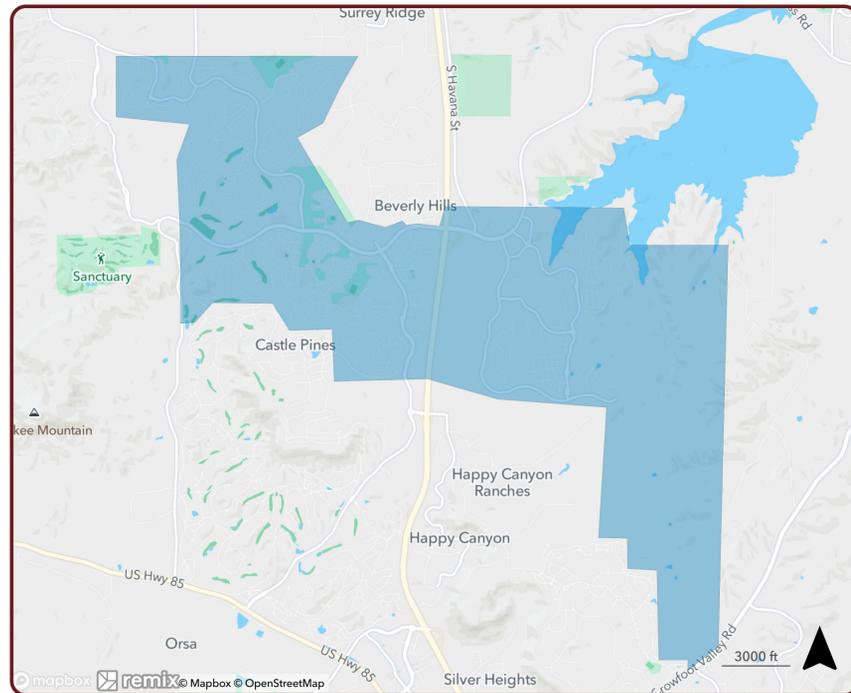
- Covers fewer passengers in between stops, so they must walk, bike, or drive to a stop
- Typically provides lower frequencies during off-peak times
- Trade-off between providing service to outlying areas and increasing frequency of service to major destinations

WHAT OTHER SERVICE OPTIONS WERE CONSIDERED?

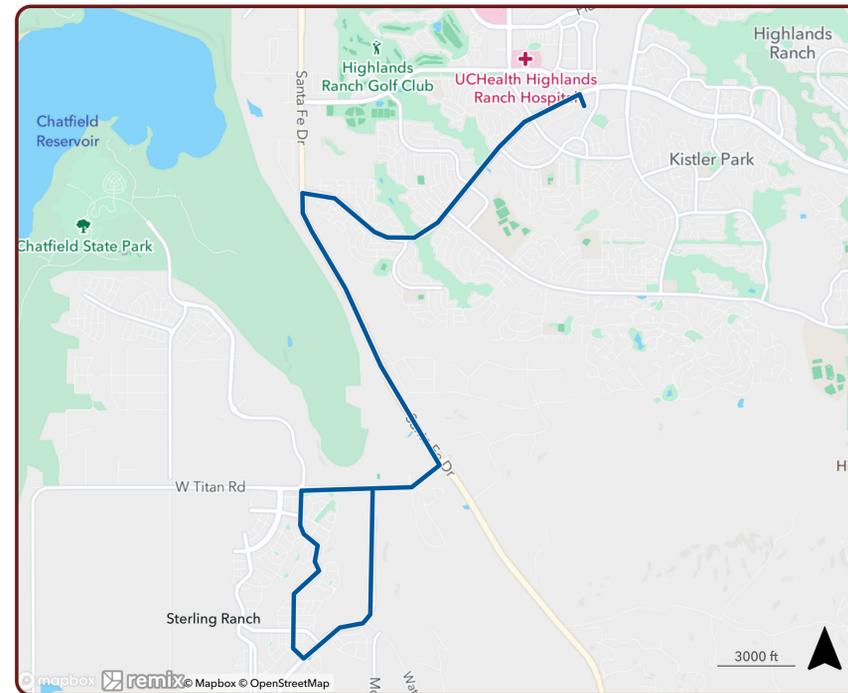
The project team investigated what the following transit options could look like. Existing travel patterns and the lack of likely origin/destination pairs make these options unlikely to be successful near-term transit solutions. However, they should be revisited in the future.

Ask a team member today for more details, or leave your thoughts and questions on a sticky note here.

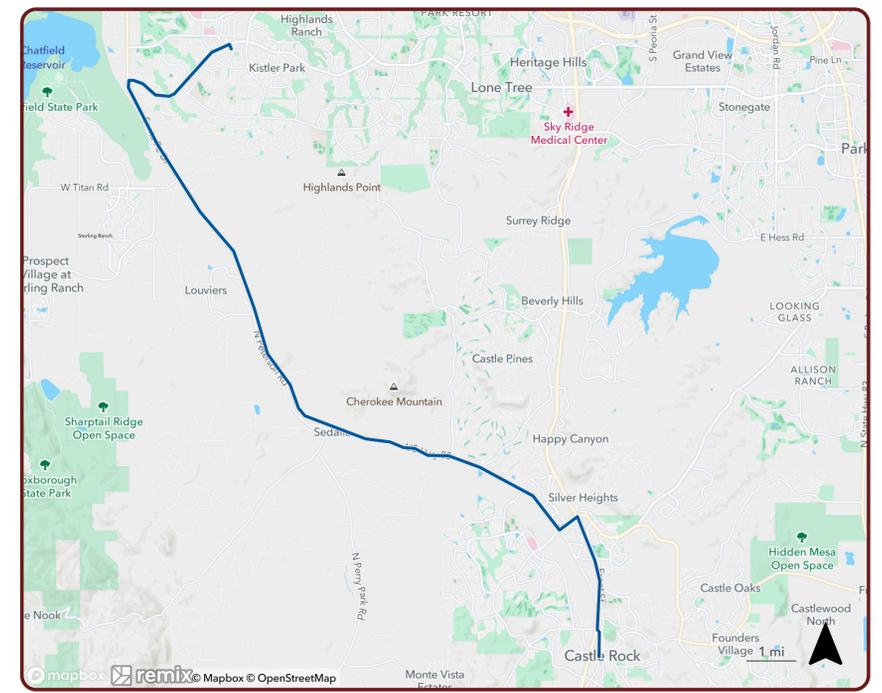
CASTLE PINES LOCAL TRANSIT



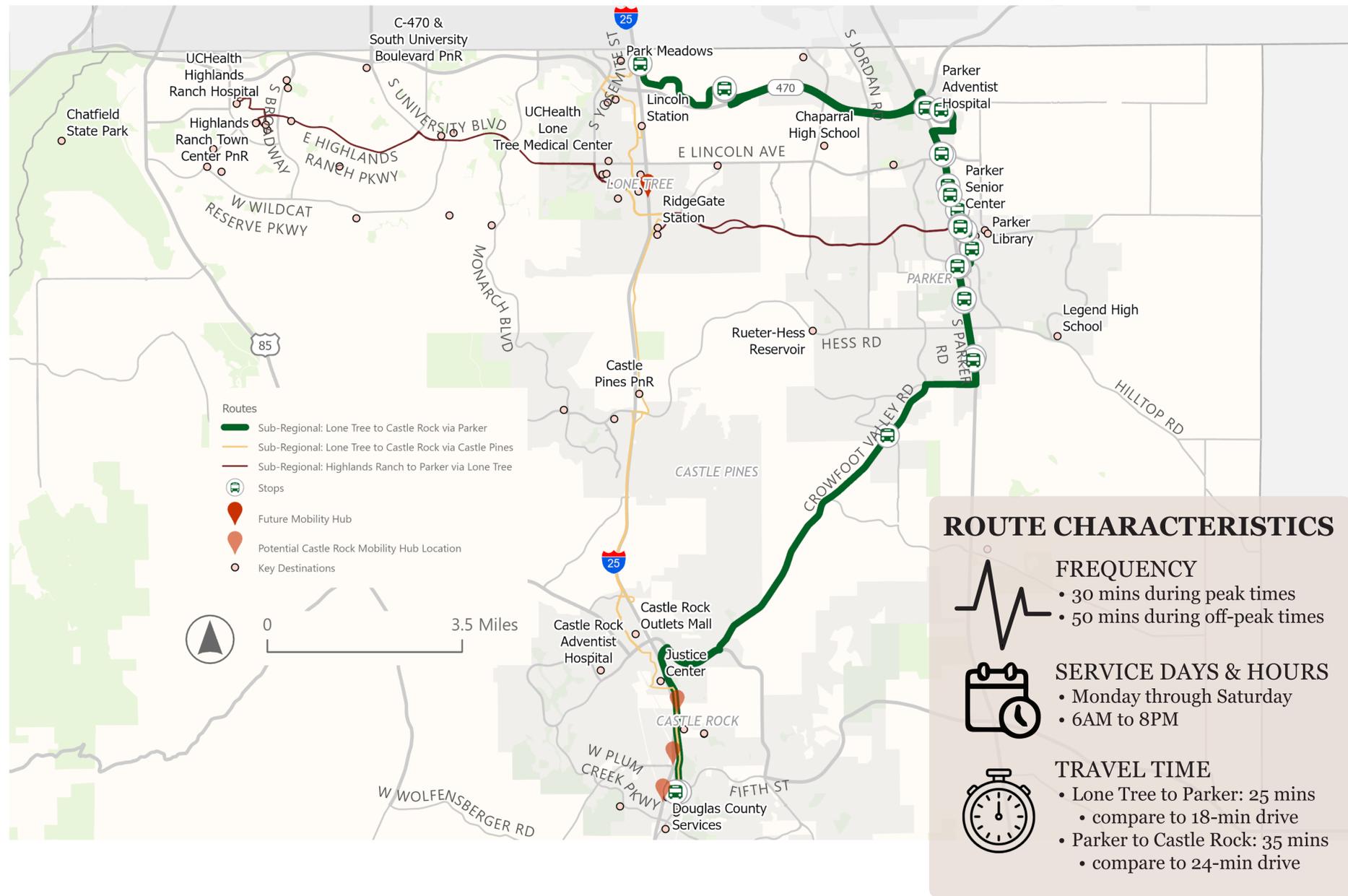
HIGHLANDS RANCH TO STERLING RANCH ROUTE



HIGHLANDS RANCH TO CASTLE ROCK VIA SEDALIA (US 85)



LONE TREE - PARKER - CASTLE ROCK SUBREGIONAL ROUTE



ROUTE CHARACTERISTICS

- FREQUENCY**
 - 30 mins during peak times
 - 50 mins during off-peak times
- SERVICE DAYS & HOURS**
 - Monday through Saturday
 - 6AM to 8PM
- TRAVEL TIME**
 - Lone Tree to Parker: 25 mins
 - compare to 18-min drive
 - Parker to Castle Rock: 35 mins
 - compare to 24-min drive

When thinking about the travel needs of people in your community, how favorably do you view the Lone Tree – Parker – Castle Rock fixed subregional route?

Place sticky notes with any route changes or comments on this route here/on the map

FAVORABLE

NEUTRAL

UNFAVORABLE

How does this route meet project goals?



HIGHLANDS RANCH - LONE TREE - PARKER SUBREGIONAL ROUTE



When thinking about the travel needs of people in your community, how favorably do you view the Highlands Ranch - Lone Tree – Parker fixed subregional route?

Place a sticky note with comments or route suggestions on this route here/on the map

FAVORABLE

NEUTRAL

UNFAVORABLE

How does this route meet project goals?

Ridership



People Served



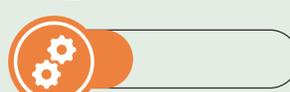
Connections



Cost Efficiency



Implementation



ROUTE CHARACTERISTICS



FREQUENCY

- 30 mins during peak times
- 60 mins during off-peak times



SERVICE DAYS & HOURS

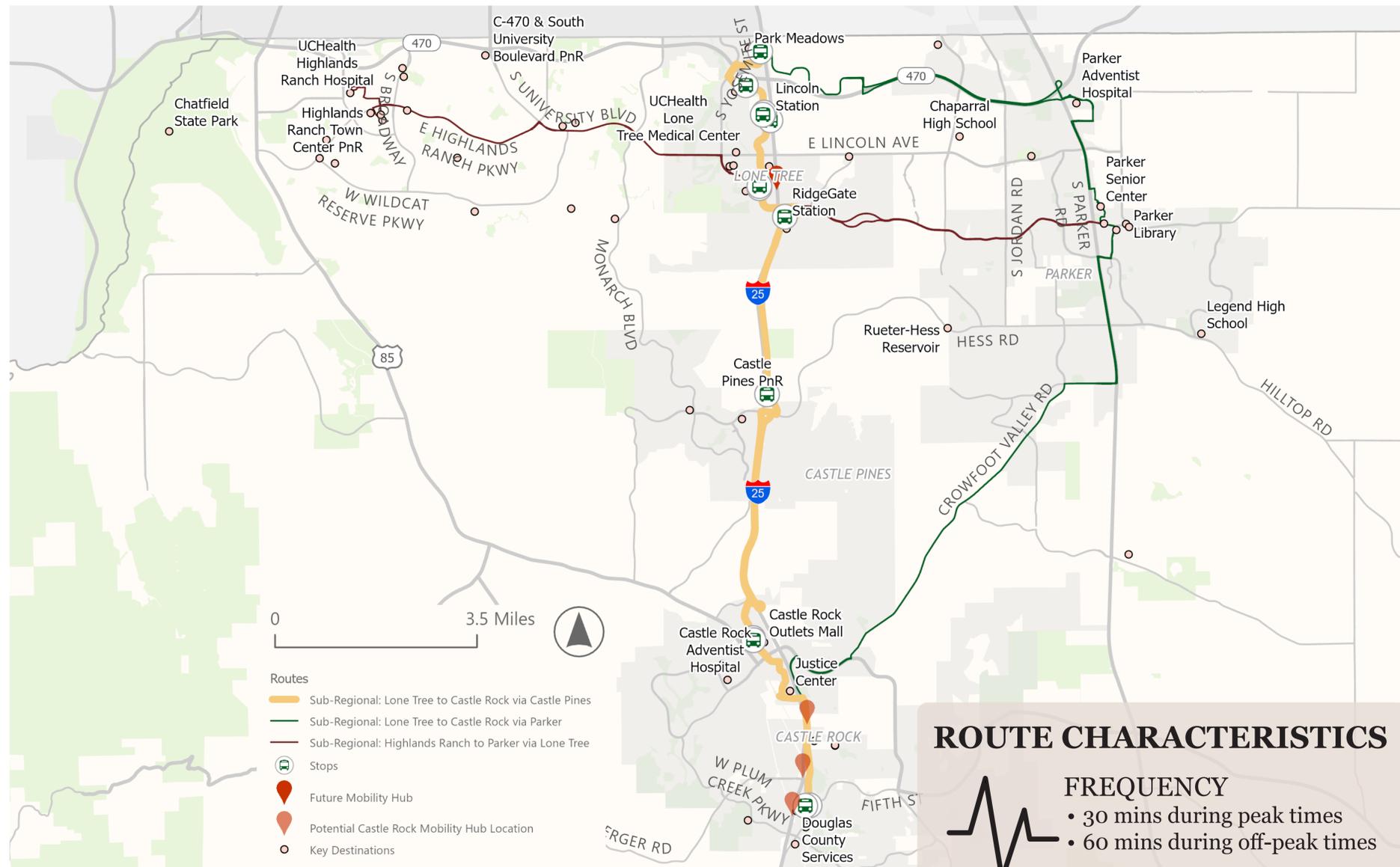
- Monday through Saturday
- 6AM to 8PM



TRAVEL TIME

- Highlands Ranch to Lone Tree: 35 mins
 - compare to 19-min drive
- Lone Tree to Parker: 20 mins
 - compare to 14-min drive

LONE TREE - CASTLE PINES - CASTLE ROCK SUBREGIONAL ROUTE



When thinking about the travel needs of people in your community, how favorably do you view the Lone Tree – Castle Pines – Castle Rock fixed subregional route?

Place a sticky note with comments or route suggestions on this route here/on the map

FAVORABLE

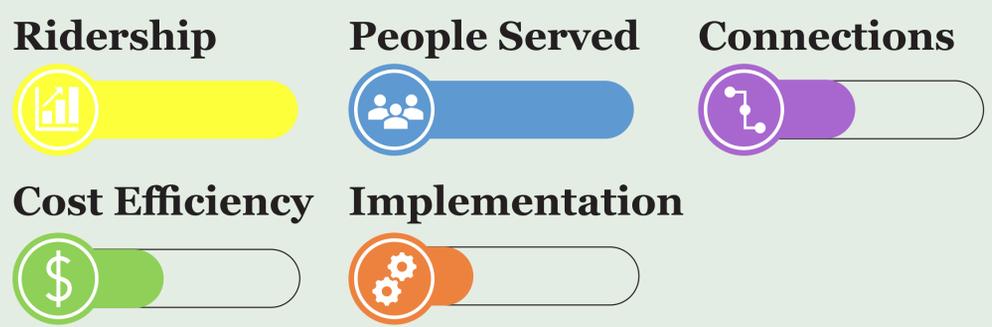
NEUTRAL

UNFAVORABLE

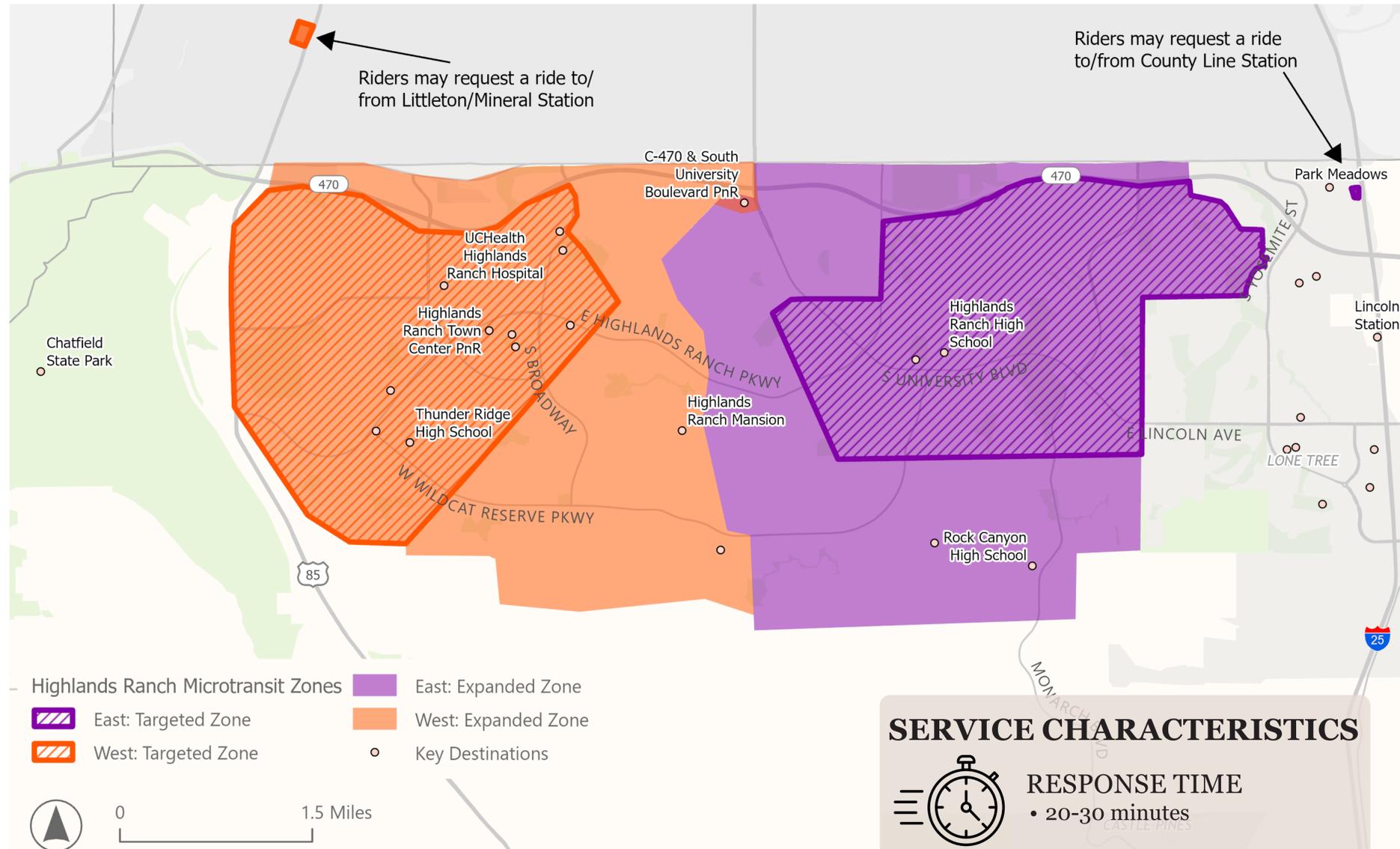
ROUTE CHARACTERISTICS

- FREQUENCY**
 - 30 mins during peak times
 - 60 mins during off-peak times
- SERVICE DAYS & HOURS**
 - Monday through Saturday
 - 6AM to 8PM
- TRAVEL TIME**
 - Lone Tree to Castle Pines: 25 mins
 - compare to 17-min drive
 - Castle Pines to Castle Rock: 25 mins
 - compare to 18-min drive

How does this route meet project goals?



HIGHLANDS RANCH LOCAL TRANSIT



When thinking about the travel needs of people in your community, how favorably do you view the Highlands Ranch microtransit option?

Place a sticky note with comments or route suggestions on this route here/on the map

FAVORABLE

NEUTRAL

UNFAVORABLE

SERVICE CHARACTERISTICS

RESPONSE TIME
• 20-30 minutes

SERVICE DAYS & HOURS
• Monday through Saturday
• 7AM to 7PM

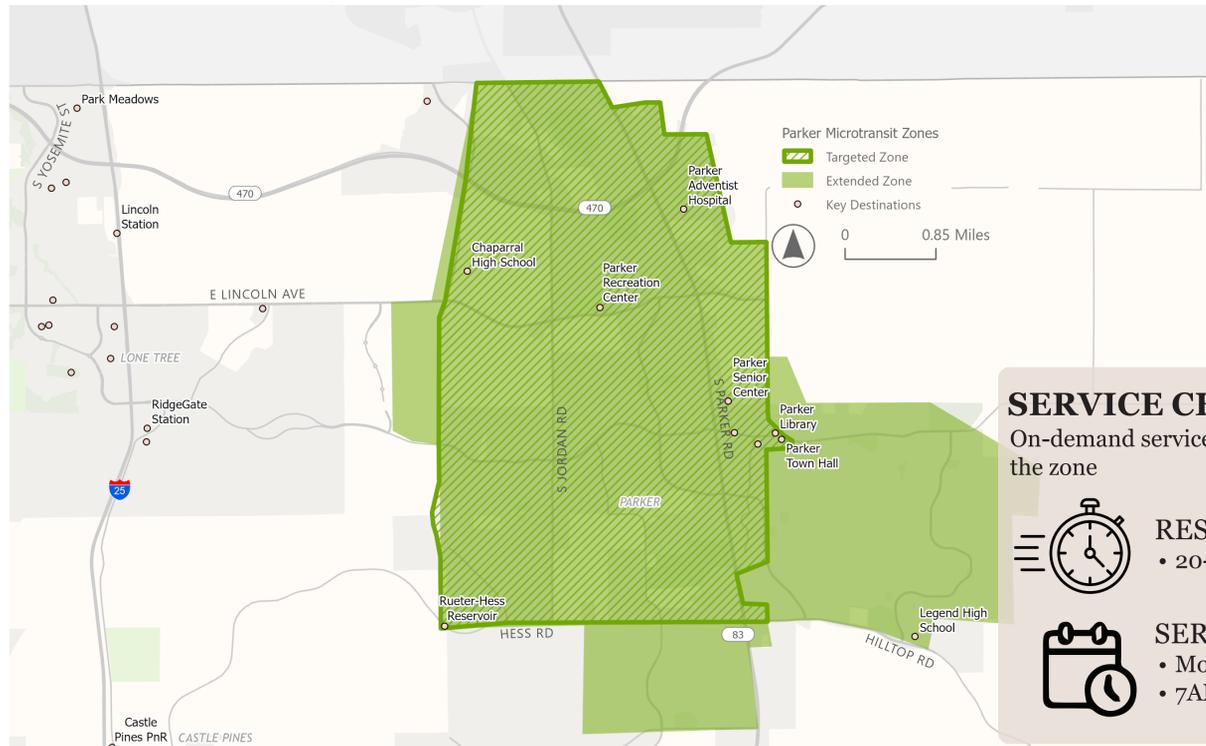
How does this microtransit zone meet project goals?

Ridership 	People Served 	Connections
Cost Efficiency 	Implementation 	

PARKER LOCAL TRANSIT



MICROTRANSIT

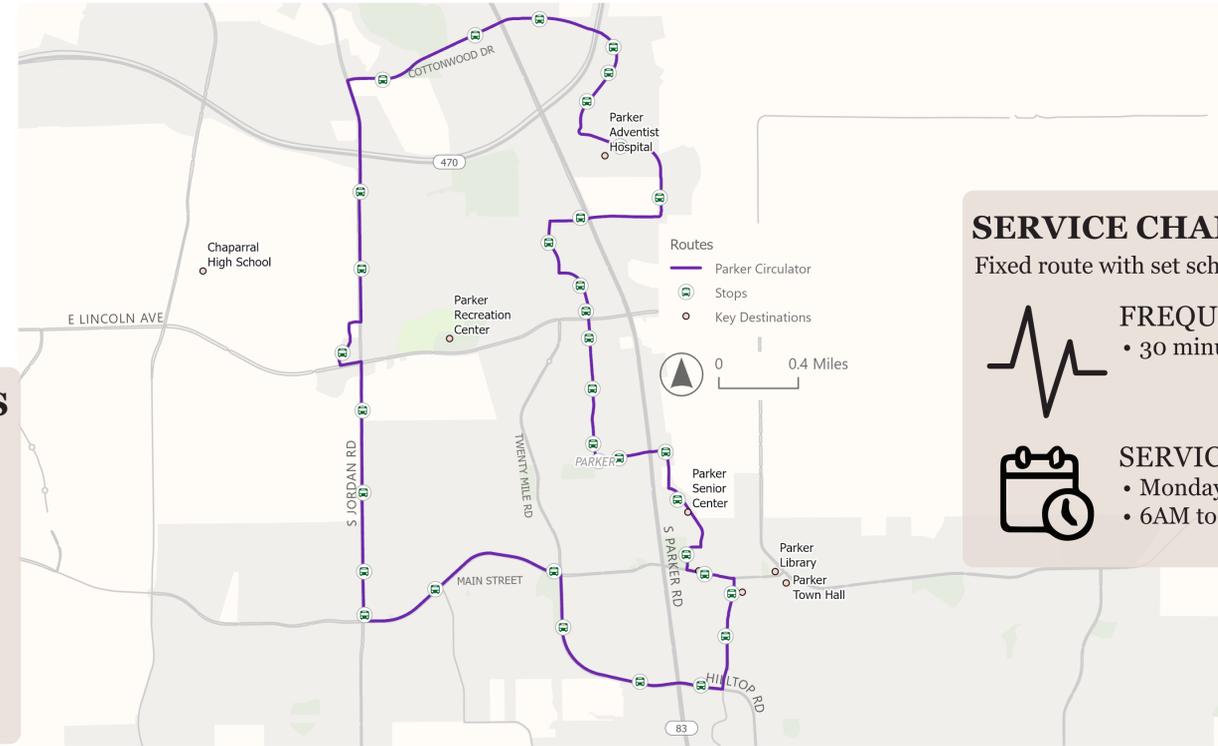


SERVICE CHARACTERISTICS
On-demand service between two points within the zone

RESPONSE TIME
• 20-30 minutes

SERVICE DAYS & HOURS
• Monday through Saturday
• 7AM to 7PM

CIRCULATOR



SERVICE CHARACTERISTICS
Fixed route with set schedule

FREQUENCY
• 30 minutes

SERVICE DAYS & HOURS
• Monday through Saturday
• 6AM to 8PM



When thinking about the travel needs of people in your community, how favorably do you view the Parker Microtransit and Circulator Options?

Place a sticky note with comments or route suggestions on this route here/on the map

FAVORABLE

NEUTRAL

UNFAVORABLE

Place a sticker on your preferred local service option

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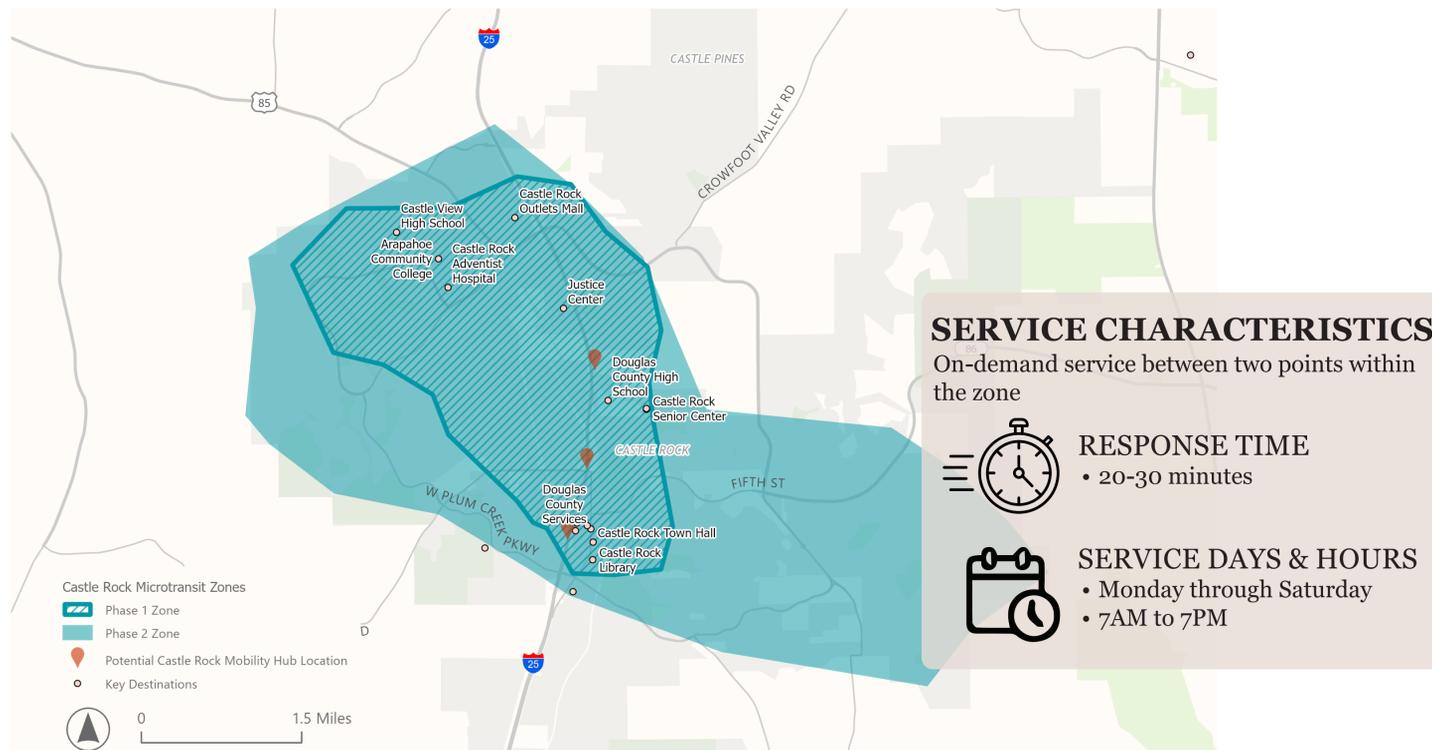
CIRCULATOR : **MICROTRANSIT**

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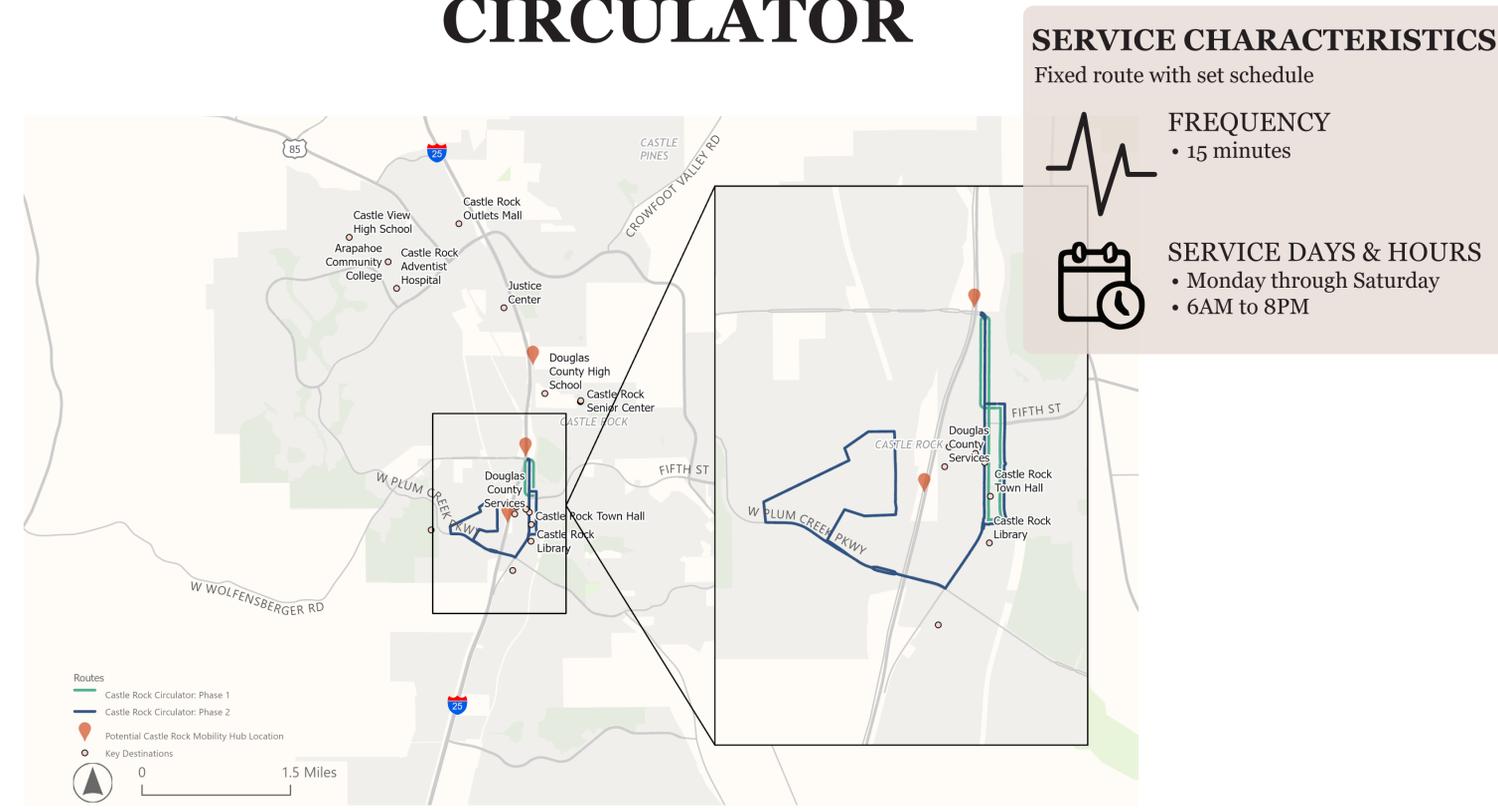
CASTLE ROCK LOCAL TRANSIT



MICROTRANSIT



CIRCULATOR



How does this microtransit zone meet project goals?



How does this route meet project goals?



When thinking about the travel needs of people in your community, how favorably do you view the Castle Rock local service option?

Place a sticky note with comments or route suggestions on this route here/on the map

FAVORABLE

NEUTRAL

UNFAVORABLE

Place a sticker on your preferred local service option

.....

CIRCULATOR : **MICROTRANSIT**

.....

SUPPORTING FEATURES

Please select the TOP THREE supporting features that may lead you to take public transit

SAFETY

(security cameras on vehicles and at bus stops, well-trained drivers)

HIGH FREQUENCY SERVICE AND RELIABLE SERVICE

(every 30 minutes or less, and high on-time performance)

TRIP PLANNING TOOLS

(e.g., transit vehicle tracker app)

COMFORTABLE TRANSIT STOPS

(bike storage, trash cans, shelters, lighting)

BIKE RACKS ON TRANSIT VEHICLES

NEAR A PARK-N-RIDE

AFFORDABLE FARE

CONNECTION TO EXISTING TRANSIT SERVICES OR CENTERS

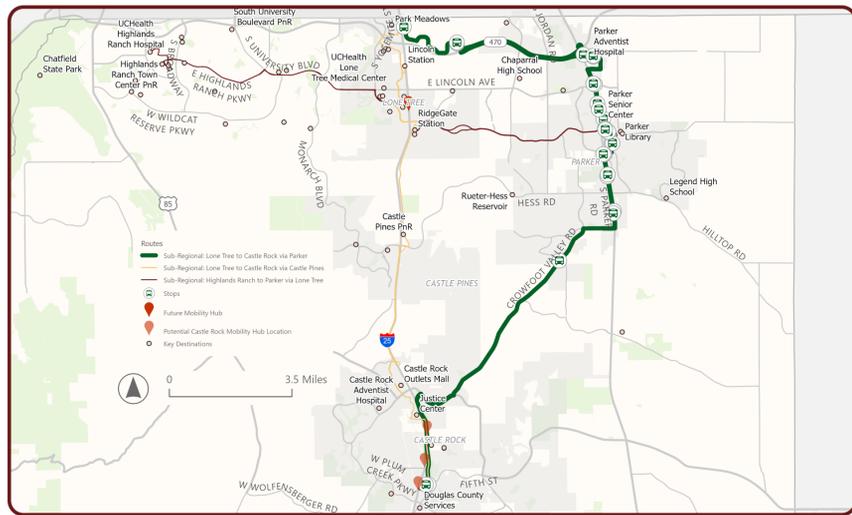
LATE NIGHT/ WEEKEND SERVICE

OTHER

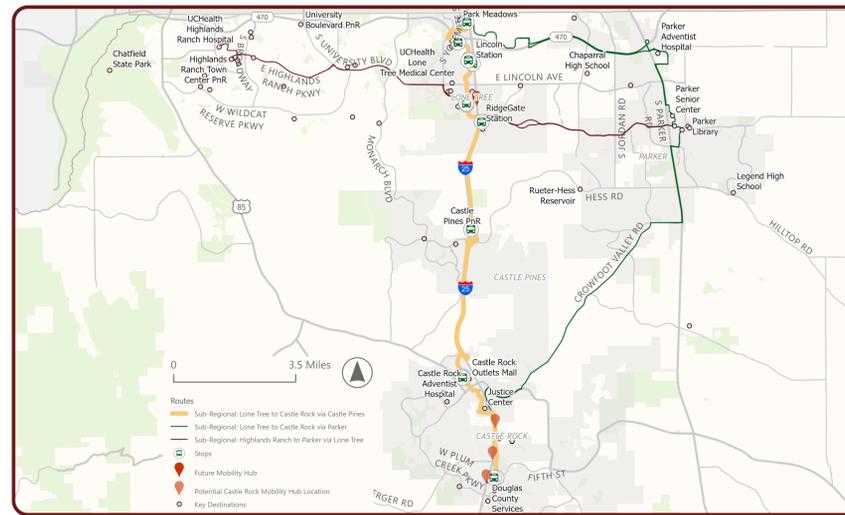
INVEST IN YOUR TOP TRANSIT OPTIONS

Recognizing that funding opportunities are limited and implementation will need to be phased, please rank your top three transit options by putting a block into the jar corresponding to each option.

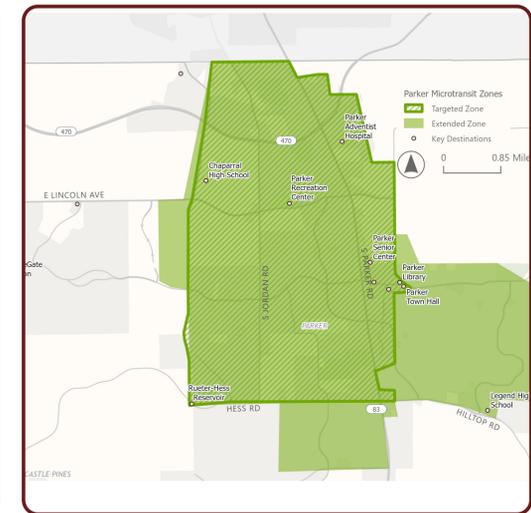
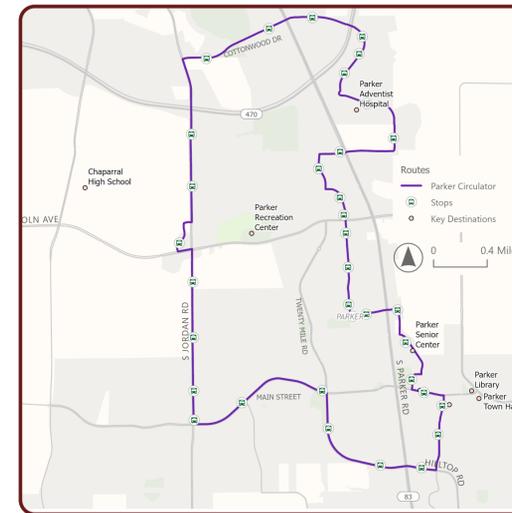
LONE TREE – PARKER – CASTLE ROCK (SUBREGIONAL FIXED ROUTE)



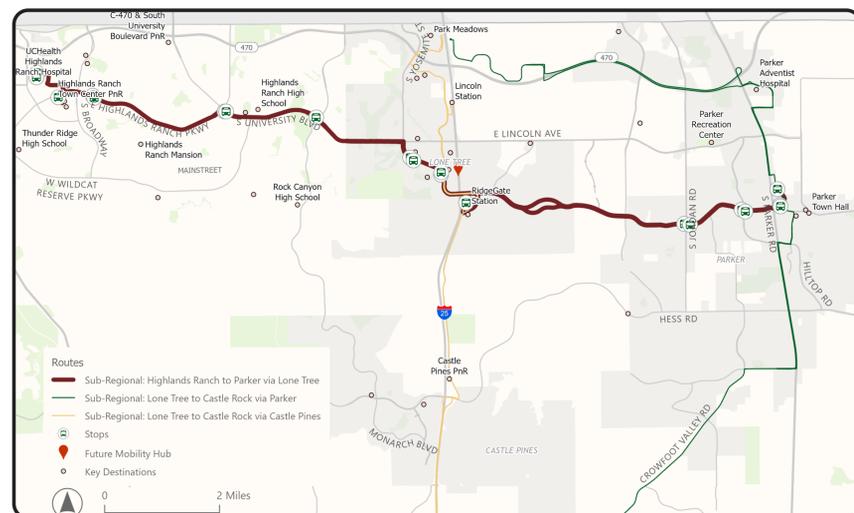
LONE TREE – CASTLE PINES – CASTLE ROCK (SUBREGIONAL FIXED ROUTE)



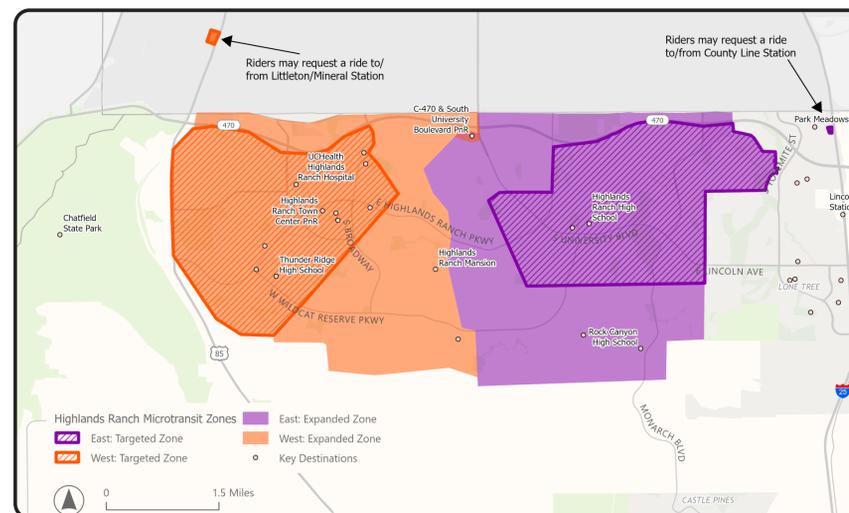
PARKER LOCAL (CIRCULATOR OR MICROTRANSIT)



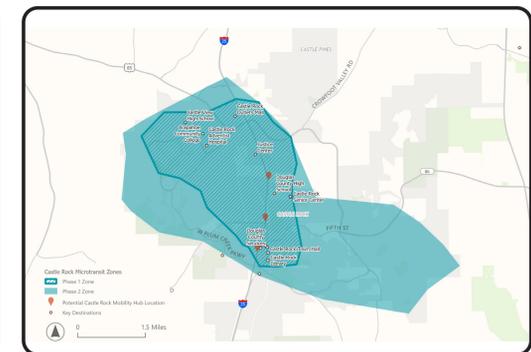
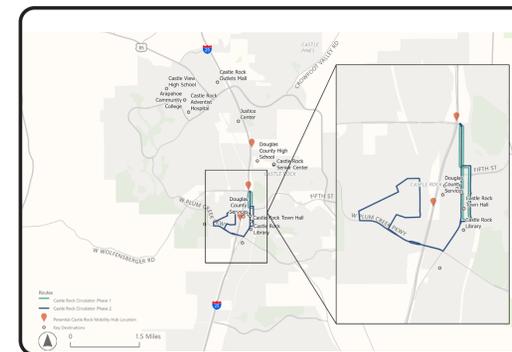
HIGHLANDS RANCH - LONE TREE – PARKER (SUBREGIONAL FIXED ROUTE)



HIGHLANDS RANCH LOCAL (MICROTRANSIT)



CASTLE ROCK LOCAL (CIRCULATOR OR MICROTRANSIT)



HOW WOULD YOU USE TRANSIT?

How often would you use public transit if it served your desired destinations? (Place a sticker under the one that applies)

DAILY

WEEKLY

MONTHLY

A COUPLE
TIMES A YEAR

NEVER

What type of trips would you most likely use a new public transit service for? (Select all that apply)

CONNECT TO EXISTING PUBLIC TRANSIT SERVICES
(RTD BUSES AND LIGHT RAIL, LONE TREE LINK ON DEMAND, ETC.)

COMMUTING TO WORK OR SCHOOL

ACCESSING ESSENTIAL SERVICES
(GROCERIES, MEDICAL APPOINTMENTS, SOCIAL SERVICES)

RECREATION
(DINING, SHOPPING, VISITING FRIENDS, EXPERIENCING PARKS AND RECREATION CENTERS)

OTHER
(PLEASE SPECIFY)

What days of the week and times are you most likely to use a public transit service? (Select all that apply)

WEEKDAYS
6 AM TO 10 AM

WEEKDAYS
10 AM TO 2 PM

WEEKDAYS
2 PM TO 7 PM

WEEKDAYS
7 PM TO MIDNIGHT

WEEKENDS
6 AM TO 10 AM

WEEKENDS
10 AM TO 2 PM

WEEKENDS
2 PM TO 7 PM

WEEKENDS
7 PM TO MIDNIGHT

OPEN HOUSE ORIENTATION



Welcome!

Today's open house allows you to visit the stations that interest you the most, ask the project team questions, and provide your feedback. There will be a 20-minute presentation mid-way through the open house.

Stations

1 Project Goals & Timeline	2 Existing Data	3 What We Heard	4 Transit Types	5 Subregional Routes	6 Local Transit	7 Supporting Elements
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Provide Your Feedback

- How could transit foster improved mobility within the study area?
- How would you rank the transit options for implementation?
- Do you have specific input that's missing?
- How often would you use public transit, for what type of trips, and when?
- Is there anything else you'd like the County to know?

Scan code to learn more about the project here:



OPEN HOUSE GOALS



Understand the community's needs, barriers, and perceptions about transit to be incorporated into proposed short-and-long term transit and multimodal projects



Foster a transparent and collaborative transportation decision-making process



Create support for the study goals and outcomes



Get feedback on identified transit opportunities



Be clear about what kind of input is sought and how it will be used